



# Rathdrum

City Council Agenda

March 11, 2020

6:00 p.m.

Location: City Council Chamber  
8047 W. Main Street  
Rathdrum, ID 83858  
(208) 687-0261

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**WELCOME-PLEASE TURN OFF CELL PHONES- Thank You.**

- 1) **CALL THE MEETING TO ORDER**
- 2) **PLEDGE OF ALLEGIANCE**
- 3) **ROLL CALL**
- 4) **AMENDMENTS TO THE AGENDA**
- 5) **ACTION ITEM: CONSENT CALENDAR APPROVAL**
  - A) Special Council Minutes of February 26, 2020
  - B) Regular and Special Bills as presented
- 6) **CEREMONIES AND REPORTS**
- 7) **VISITORS COMMENTS – An opportunity to address concerns not on the agenda (No formal action allowed – 3-minute time limit per issue) issues may be placed on subsequent agenda. Comments regarding performance by city employees are inappropriate at this time and should be directed to the Mayor, either by subsequent appointment or after tonight’s meeting, if time permitting. Repeated comments of the same or similar topic will be considered out of order and not allowed.**

## 8) OLD BUSINESS

## 9) NEW BUSINESS

- A) **ACTION ITEM:** Consideration of the Phone System Upgrade
- B) **ACTION ITEM:** Consideration of Memorandum of Understanding and Construction Cooperative Agreement with the Idaho Department of Transportation for SH41 widening project from Hayden Ave to Boekel Rd.
- C) **ACTION ITEM:** Consideration of the Professional Services Agreement with Lake City Engineering for Intersection Improvements at Meyer Road – Lancaster Road
- D) **ACTION ITEM:** Consideration of the acceptance for Grant of Right-of-Way for unnamed roadway East of Corbin Crossing South
- E) **ACTION ITEM:** Consideration of the Westwood Pine Preliminary Long Plat

## 11)STAFF REPORTS

- A) Public Works Reporting
- B) Police Reporting
- C) Park and Recreation Reporting
- D) City Administrator

## 12)MAYOR’S REPORT/APPOINTMENTS

## 13)COUNCIL REPORTS

## 14)ADJOURN

**Rathdrum City Hall is an ADA accessible building. Assistance for persons with disabilities will be provided upon 24-hours’ notice prior to the meeting.**



# *Council Calendar*

## *March 11<sup>th</sup>, 2020*

March 11	6:00 pm	Regular Council Meeting
March 12	10:30 am	Senior Center Meeting
March 12	1:30 pm	KMPO
March 12	6:00 pm	Citizen's Academy
March 13	8:00 am	Council Payday
March 18	6:00 pm	Planning & Zoning Commission
March 19	11:50 am	Chamber Luncheon
March 19	6:00 pm	Citizen's Academy
March 24	8:00 am	KCATT
March 25	6:00 pm	Regular Council Meeting
March 26	6:00 pm	Citizen's Academy





# Rathdrum

Special Council Minutes

February 26, 2020

5:30 p.m.

Location: City Council Chamber  
8047 W. Main Street  
Rathdrum, ID 83858  
(208) 687-0261



**WELCOME-PLEASE TURN OFF CELL PHONES- Thank You.**

## 1) CALL THE MEETING TO ORDER

Meeting called to order at 5:35 pm

## 2) PLEDGE OF ALLEGIANCE

## 3) ROLL CALL

**PRESENT:** Councilor Hill, Councilor Laws, Councilor Rickard, Councilor Adams and Mayor Holmes.

**ABSENT:**

**STAFF:** City Administrator Duce, City Clerk Halligan, City Planner Siess and City Attorney Herrington

## 4) AMENDMENTS TO THE AGENDA

## 5) **ACTION ITEM:** CONSENT CALENDAR APPROVAL

- A) Council Minutes of February 12, 2020
- B) Regular and Special Bills as presented
- C) Beer & Wine License for Bi-Mart

City Administrator gave a brief presentation on the consent calendar

Councilor Rickard made a MOTION that we approve the Consent Calendar as presented. Motion seconded by Councilor Hill.

**AYES:** Councilor Rickard  
Councilor Hill  
Councilor Laws  
Councilor Adams

**NAYES:** None

**ABSENT:**

Passed by all

- 6) VISITORS COMMENTS – An opportunity to address concerns not on the agenda (No formal action allowed – 3-minute time limit per issue) issues may be placed on subsequent agenda. Comments regarding performance by city employees are inappropriate at this time and should be directed to the Mayor, either by subsequent appointment or after tonight’s meeting, if time permitting. Repeated comments of the same or similar topic will be considered out of order and not allowed.**

Kelly Guzman  
13900-2 Lauren Loop  
Rathdrum, ID

I am here tonight to ask a few questions that are relative to the Kootenai County Emergency preparedness survey. Does the City have an Emergency plan in the case of a disaster?

Mayor Holmes stated that we do have an Emergency Plan in place but we do need to readdress it because we have a lot of new staff. The mayor is the only person that can declare a disaster.

## **7) WORKSHOP – PLANNING AND ZONING TRAINING**

Will Herrington  
Lawyer and Planner

Mr. Herrington gave a power point presentation on the Understanding your responsibilities and the different processes in Planning and Zoning.

Mr. Herrington went over these topics:

- Approaching your responsibilities
- Differences in two types of hearings
- Process and outcomes – Legislative
- Process and outcomes – Quasi Judicial

- Land use issues you will encounter

## 8) ADJOURN

Meeting was adjourned at 8:10 pm

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Vic Holmes, Mayor

Attest:

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Sherri L Halligan, City Clerk

**Rathdrum City Hall is an ADA accessible building. Assistance for persons with disabilities will be provided upon 24-hours' notice prior to the meeting.**

## Report Criteria:

Detail report.  
Invoices with totals above \$0.00 included.  
Only unpaid invoices included.

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
<b>10-218600</b>							
2445	STATE INSURANCE FUND	22434586	Audit premium adjustment	02/24/2020	4,851.00	.00	
Total 10-218600:					4,851.00	.00	
<b>10-415-310</b>							
134	AMAZON CAPITAL SERVICES	1XMQ-76XK-9	Dry erase markers, wipes	03/02/2002	32.00	.00	
2440	STAPLES ADVANTAGE	3439396991	Folders, legal folders	02/15/2020	29.91	.00	
2440	STAPLES ADVANTAGE	3439396992	Credit on inv 3438847844	02/13/2020	4.44	.00	
2518	SUPER ONE FOODS	07-2403173	Creamer	02/27/2020	4.67	.00	
2815	VISA	MT7773FEB20	Feb credit card pymt MT	01/28/2020	48.19	.00	
Total 10-415-310:					110.33	.00	
<b>10-415-320</b>							
546	CRYSTAL SPRINGS	10077301-022	Cooler rent & water	02/29/2020	113.81	.00	
Total 10-415-320:					113.81	.00	
<b>10-415-370</b>							
1240	J.THAYER COMPANY, INC	1424390-0	Envelopes	01/16/2020	37.45	.00	
Total 10-415-370:					37.45	.00	
<b>10-415-510</b>							
134	AMAZON CAPITAL SERVICES	13FL-H4L7-TM	Whiteboard,dry erase whiteboard,	03/05/2020	87.49	.00	
75	TIME WARNER CABLE	002706402252	Internet fee	02/25/2020	132.50	.00	
2810	VERIZON WIRELESS, BELLEVU	9849157182	Cell phones -CH	02/25/2020	125.10	.00	
Total 10-415-510:					345.09	.00	
<b>10-415-796</b>							
2271	ROYAL BUSINESS SYSTEMS-T	26511865	Copiers	02/26/2020	416.81	.00	
Total 10-415-796:					416.81	.00	
<b>10-416-423</b>							
1977	POST FALLS LAW	209004	Prof services	02/28/2020	4,500.00	.00	
Total 10-416-423:					4,500.00	.00	
<b>10-416-429</b>							
283	BENEFITS EXCHANGE NORTH	3741	Admin fee	02/03/2020	42.00	.00	
Total 10-416-429:					42.00	.00	
<b>10-419-421</b>							
161	ANDERSON BROS CPA'S PA	1381	Audit progreess billing	02/29/2020	833.33	.00	
Total 10-419-421:					833.33	.00	
<b>10-419-690</b>							
543	CREATIVE TOUCH FLORAL	3220	Flowers- Kennedy	03/02/2020	57.00	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
Total 10-419-690:					57.00	.00	
<b>10-421-310</b>							
134	AMAZON CAPITAL SERVICES	1XMQ-76XK-9	Dry erase markers, wipes	03/02/2002	22.58	.00	
2440	STAPLES ADVANTAGE	3439946095	Cups, markers, dvd, post-its	02/22/2020	133.16	.00	
Total 10-421-310:					155.74	.00	
<b>10-421-320</b>							
2328	SERIGHT'S ACE HARWARE 3	13703/3	Nozzle twist	02/26/2020	7.19	.00	
2328	SERIGHT'S ACE HARWARE 3	13720/3	Paper towel holder	02/27/2020	6.83	.00	
2815	VISA	ES1391FEB20	Feb credit card pymt RPD	02/28/2020	41.27	.00	
Total 10-421-320:					55.29	.00	
<b>10-421-325</b>							
2310	SATURDAY NIGHT, INC.	971099	Shirts	02/10/2020	43.71	.00	
2310	SATURDAY NIGHT, INC.	97272	Shirts	02/25/2020	273.57	.00	
Total 10-421-325:					317.28	.00	
<b>10-421-432</b>							
78	A DRUG FREE ALLIANCE	32087RPD	Clinic fees-Brown & Allen	02/17/2020	100.00	.00	
Total 10-421-432:					100.00	.00	
<b>10-421-480</b>							
1022	IACP	0103070	Annual dues Tomi McLean	01/24/2019	190.00	.00	
1025	IDAHO CHIEFS of POLICE ASSO	14/2020	Membership for sworn officers	01/16/2020	400.00	.00	
2704	TRANSUNION RISK ALTERNATI	FEB1-FEB28	People search	02/28/2020	50.00	.00	
Total 10-421-480:					640.00	.00	
<b>10-421-510</b>							
1979	POWERNET GLOBAL COMMUNI	41874515	Long distance service	02/27/2020	103.08	.00	
2810	VERIZON WIRELESS, BELLEVU	9849157182	Cell phones -PD	02/25/2020	851.68	.00	
Total 10-421-510:					954.76	.00	
<b>10-421-520</b>							
240	AVISTA UTILITIES	6538920000FE	GAS & ELECTRIC	02/19/2020	381.11	.00	
240	AVISTA UTILITIES	8446740000FE	GAS & ELECTRIC	02/19/2020	46.33	.00	
1370	KOOTENAI COUNTY SOLID WA	84744	Dumpster 2 yds	01/31/2020	52.05	.00	
Total 10-421-520:					479.49	.00	
<b>10-421-610</b>							
1890	PERFECTION TIRE # 38	1-159499	Lube, mount & balanced tires	02/24/2020	220.50	.00	
1890	PERFECTION TIRE # 38	1-159622	Front brakes & repairs 2014 Chev	03/02/2020	1,403.49	.00	
Total 10-421-610:					1,623.99	.00	
<b>10-421-760</b>							
790	GALLS PARENT HOLDINGS,LLC	01510048	Training suit	02/24/2020	1,440.61	.00	
Total 10-421-760:					1,440.61	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
<b>10-421-770</b>							
134	AMAZON CAPITAL SERVICES	1TQF-KVH4-F	Car charger	02/26/2020	25.80	.00	
Total 10-421-770:					25.80	.00	
<b>10-421-796</b>							
2271	ROYAL BUSINESS SYSTEMS-T	26511865	Copiers	02/26/2020	416.82	.00	
Total 10-421-796:					416.82	.00	
<b>10-421-797</b>							
1239	J&R ELECTRONICS	046219	Business internet monthly access	02/20/2020	162.50	.00	
Total 10-421-797:					162.50	.00	
<b>10-421-850</b>							
2115	RATHDRUM ANIMAL CLINIC	225658	Shots for Atara	02/19/2020	409.19	.00	
2115	RATHDRUM ANIMAL CLINIC	225852	Shots for Liberty	02/24/2020	34.70	.00	
2328	SERIGHT'S ACE HARWARE 3	12175/3	Dog food	01/03/2019	98.98	.00	
Total 10-421-850:					542.87	.00	
<b>10-423-310</b>							
134	AMAZON CAPITAL SERVICES	1XMQ-76XK-9	Dry erase markers, wipes	03/02/2002	22.57	.00	
1240	J.THAYER COMPANY, INC	1435178-0	Ink cartridges	02/25/2020	202.96	.00	
2815	VISA	KJ0839FEB20	Feb credit card charges KJ	02/28/2020	58.51	.00	
Total 10-423-310:					284.04	.00	
<b>10-423-330</b>							
270	BANNER FUEL	1-019582	Feb fuel PW	02/29/2020	2,158.60	.00	
960	HICO COUNTRY STORE, INC.	113337	Mar fuel PW	03/03/2020	21.86	.00	
Total 10-423-330:					2,180.46	.00	
<b>10-423-510</b>							
2810	VERIZON WIRELESS, BELLEVU	9849157182	Cell phones -PW	02/25/2020	585.84	.00	
Total 10-423-510:					585.84	.00	
<b>10-423-520</b>							
240	AVISTA UTILITIES	2907930000FE	GAS & ELECTRIC	02/19/2020	779.83	.00	
240	AVISTA UTILITIES	3797170000FE	Comfort billing-Chamber	02/19/2020	60.00	.00	
240	AVISTA UTILITIES	8475570000FE	GAS & ELECTRIC	02/19/2020	376.08	.00	
1370	KOOTENAI COUNTY SOLID WA	80608JAN20	Garbage dumpster	01/31/2020	351.35	.00	
1370	KOOTENAI COUNTY SOLID WA	81011JAN20	Garbage dumpster	01/31/2020	26.03	.00	
920	NORTHWEST WASTE & RECYC	406886	6yd fl 2xwk	02/29/2020	145.10	.00	
920	NORTHWEST WASTE & RECYC	406888	2yd fl 1xw	02/29/2020	36.50	.00	
Total 10-423-520:					1,774.89	.00	
<b>10-423-770</b>							
2815	VISA	KJ0839FEB20	Feb credit card charges KJ	02/28/2020	139.98	.00	
Total 10-423-770:					139.98	.00	
<b>10-423-920</b>							
2271	ROYAL BUSINESS SYSTEMS-T	26511865	Copier	02/26/2020	416.82	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
Total 10-423-920:					416.82	.00	
<b>10-424-429</b>							
480	COEUR D'ALENE PRESS, INC.	100368322-229	Public Hearing P & Z LGL 3223	02/29/2020	116.99	.00	
480	COEUR D'ALENE PRESS, INC.	100368328-229	Public Hearing P & Z LGL 3224	02/29/2020	130.83	.00	
Total 10-424-429:					247.82	.00	
<b>10-426-320</b>							
2815	VISA	KJ0839FEB20	Feb credit card pymt KJ	02/28/2020	315.33	.00	
Total 10-426-320:					315.33	.00	
<b>10-431-320</b>							
394	CD'A METALS	574270	Tubing	02/25/2020	59.29	.00	
1720	Norco	28696875	Cyl rent	02/29/2020	51.62	.00	
1726	NORTH 40 OUTFITTERS	30677/E	Batteries, havis plugg & connector	02/20/2020	77.65	.00	
1815	O'REILLY AUTO PARTS	3930-480760	V-belt	02/25/2020	24.24	.00	
1830	OXARC, INC.	60588043	Acetylene	02/29/2020	11.57	.00	
2144	RATHDRUM TRADING POST HA	5738/1	Paint	02/27/2020	2.33	.00	
2144	RATHDRUM TRADING POST HA	5759/1	Flex wrench	03/02/2020	18.88	.00	
2328	SERIGHT'S ACE HARWARE 3	13438/3	Grade stakes	01/30/2020	23.29	.00	
2328	SERIGHT'S ACE HARWARE 3	13467/3	Molding tape	02/04/2020	5.39	.00	
2328	SERIGHT'S ACE HARWARE 3	13682/3	Black oxide dibt	02/24/2020	59.54	.00	
2328	SERIGHT'S ACE HARWARE 3	13715/3	Ace shovel, gloves	02/27/2020	34.18	.00	
2328	SERIGHT'S ACE HARWARE 3	13759/3	Bucket, dust pan	03/02/2020	8.97	.00	
2328	SERIGHT'S ACE HARWARE 3	7903/3	Hand saw, pruner	07/26/2018	73.54	.00	
Total 10-431-320:					450.49	.00	
<b>10-431-525</b>							
240	AVISTA UTILITIES	0204138305FE	ELECTRIC	02/19/2020	40.36	.00	
240	AVISTA UTILITIES	0395820000FE	Street lights	02/19/2020	33.68	.00	
240	AVISTA UTILITIES	0432070000FE	ELECTRIC	02/19/2020	56.53	.00	
240	AVISTA UTILITIES	2219530000FE	Street lights	02/19/2020	13.00	.00	
240	AVISTA UTILITIES	2756400000FE	Street lights	02/28/2020	6,651.30	.00	
240	AVISTA UTILITIES	2820160000FE	Street lights	02/19/2020	13.13	.00	
240	AVISTA UTILITIES	4527920000F	Street lights	02/19/2020	37.91	.00	
240	AVISTA UTILITIES	5798260000FE	ELECTRIC	02/19/2020	70.88	.00	
240	AVISTA UTILITIES	5828150000FE	Street lights	02/19/2020	14.14	.00	
240	AVISTA UTILITIES	6732020000FE	Street lights	02/19/2020	32.12	.00	
240	AVISTA UTILITIES	6852400000FE	Street lights	02/19/2020	13.23	.00	
240	AVISTA UTILITIES	7122810000FE	Street lights	02/19/2020	14.14	.00	
240	AVISTA UTILITIES	7896220000FE	ELECTRIC	02/19/2020	14.12	.00	
1385	KOOTENAI ELECTRIC	1488980FEB20	Street lights	02/20/2020	135.00	.00	
1385	KOOTENAI ELECTRIC	1587962FEB20	Street lights	02/20/2020	322.88	.00	
1385	KOOTENAI ELECTRIC	1800084FEB20	Street lights	02/28/2020	294.61	.00	
1385	KOOTENAI ELECTRIC	1831880FEB20	Street lights	02/28/2020	44.28	.00	
1385	KOOTENAI ELECTRIC	1832815FEB20	Street lights	02/28/2020	647.66	.00	
1385	KOOTENAI ELECTRIC	1837059FEB20	Street lights	02/28/2020	90.78	.00	
1385	KOOTENAI ELECTRIC	1844717FEB20	Street lights	02/20/2020	573.15	.00	
Total 10-431-525:					9,112.90	.00	
<b>10-431-610</b>							
1505	LES SCHWAB TIRES	12700311537	Winter changeover	02/25/2020	79.96	.00	
1505	LES SCHWAB TIRES	12700311885	Fkat repair	02/28/2020	37.00	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
1650	NAPA AUTO STORE, CDA, INC.	896568	Credit memo	02/13/2020	222.62-	.00	
1650	NAPA AUTO STORE, CDA, INC.	897724	Brake pads, evap canister	02/25/2020	222.38	.00	
1815	O'REILLY AUTO PARTS	3930-480843	Muffler	02/26/2020	77.25	.00	
2369	SIX ROBBLEES' INC	5-870608-1	SAM shoe assy	03/02/2020	154.96	.00	
Total 10-431-610:					348.93	.00	
<b>10-431-846</b>							
2910	WESTERN STATES EQUIPMENT	IN001229238	Caterpillar S WNg	02/19/2020	19,355.00	.00	
Total 10-431-846:					19,355.00	.00	
<b>10-435-310</b>							
134	AMAZON CAPITAL SERVICES	1XMQ-76XK-9	Dry erase markers, wipes	03/02/2002	22.57	.00	
2815	VISA	ES1391FEB20	Feb credit card pymt ES	02/28/2020	126.57	.00	
Total 10-435-310:					149.14	.00	
<b>10-435-330</b>							
960	HICO COUNTRY STORE, INC.	113425	Feb fuel PR	02/19/2020	30.54	.00	
960	HICO COUNTRY STORE, INC.	113426	Feb fuel PR	02/21/2020	89.44	.00	
960	HICO COUNTRY STORE, INC.	113427	Feb fuel PR	02/24/2020	38.40	.00	
Total 10-435-330:					158.38	.00	
<b>10-435-420</b>							
1913	PINNACLE INVESTIGATIONS C	185693 TAYLO	Package-Taylor	02/25/2020	38.50	.00	
1913	PINNACLE INVESTIGATIONS C	185695	Package-Sarian	02/25/2020	45.50	.00	
2815	VISA	ES1391FEB20	Feb credit card charges ES	02/28/2020	25.00	.00	
Total 10-435-420:					109.00	.00	
<b>10-435-480</b>							
2815	VISA	ES1391FEB20	Feb credit card pymt ES	02/28/2020	73.44	.00	
Total 10-435-480:					73.44	.00	
<b>10-435-510</b>							
787	FRONTIER	2086872399FE	Tele	02/25/2020	299.60	.00	
2810	VERIZON WIRELESS, BELLEVU	9849157182	Cell phones -PR	02/25/2020	708.45	.00	
Total 10-435-510:					1,008.05	.00	
<b>10-435-520</b>							
240	AVISTA UTILITIES	3088650000FE	ELECTRIC	02/19/2020	281.97	.00	
Total 10-435-520:					281.97	.00	
<b>10-435-720</b>							
2144	RATHDRUM TRADING POST HA	5771/1	Toilet paper holder, steel wool	03/03/2020	17.62	.00	
2328	SERIGHT'S ACE HARWARE 3	13761/3	Hinge	03/02/2020	24.29	.00	
Total 10-435-720:					41.91	.00	
<b>10-435-920</b>							
2271	ROYAL BUSINESS SYSTEMS-T	26511865	Copiers	02/26/2020	416.82	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
Total 10-435-920:					416.82	.00	
<b>10-438-320</b>							
2144	RATHDRUM TRADING POST HA	5685/1	Key	02/19/2020	3.60	.00	
2144	RATHDRUM TRADING POST HA	5692/1	Glass cleaner	02/20/2020	6.99	.00	
2144	RATHDRUM TRADING POST HA	5698/1	Glass cleaner	02/20/2020	2.69	.00	
2144	RATHDRUM TRADING POST HA	5710/1	Screws	02/24/2020	.20	.00	
2144	RATHDRUM TRADING POST HA	5741/1	Cable tie	02/27/2020	8.98	.00	
2144	RATHDRUM TRADING POST HA	5746/1	Cable tie	02/28/2020	13.49	.00	
Total 10-438-320:					35.95	.00	
<b>10-438-330</b>							
270	BANNER FUEL	1-019582PR	Feb fuel PR	02/28/2020	124.28	.00	
Total 10-438-330:					124.28	.00	
<b>10-438-510</b>							
2815	VISA	MT7773FEB20	Feb credit card charges MT	01/28/2020	65.62	.00	
Total 10-438-510:					65.62	.00	
<b>10-438-520</b>							
240	AVISTA UTILITIES	0030160000FE	Area light & electric	02/19/2020	33.38	.00	
240	AVISTA UTILITIES	0126820000FE	ELECTRIC	02/19/2020	26.71	.00	
240	AVISTA UTILITIES	2491230000FE	ELECTRIC	02/19/2020	13.30	.00	
240	AVISTA UTILITIES	3476550000FE	ELECTRIC	02/19/2020	13.25	.00	
240	AVISTA UTILITIES	3706340000FE	ELECTRIC	02/19/2020	14.61	.00	
240	AVISTA UTILITIES	3857440000FE	ELECTRIC	02/19/2020	32.41	.00	
240	AVISTA UTILITIES	4305510000FE	ELECTRIC	02/19/2020	15.99	.00	
240	AVISTA UTILITIES	4552300000FE	Area light & electric	02/20/2020	123.90	.00	
240	AVISTA UTILITIES	5030160000FE	Area Light	02/19/2020	31.71	.00	
240	AVISTA UTILITIES	5323310000FE	ELECTRIC	02/19/2020	13.13	.00	
240	AVISTA UTILITIES	9027800000FE	ELECTRIC	02/19/2020	29.26	.00	
1239	J&R ELECTRONICS	046219	Business internet monthly access	02/20/2020	162.50	.00	
Total 10-438-520:					510.15	.00	
<b>10-438-620</b>							
1505	LES SCHWAB TIRES	12700310975	Mower tube	02/18/2020	17.99	.00	
2815	VISA	ES1391FEB20	Feb credit card charges ES	02/28/2020	354.14	.00	
Total 10-438-620:					372.13	.00	
<b>10-438-780</b>							
2311	SAWYER PLUMBING LLC	309-10953	Replaced heaters, thermostats, h	02/24/2020	457.54	.00	
Total 10-438-780:					457.54	.00	
<b>10-439-320</b>							
1530	LOWE'S COMPANIES, INC	664841	Cable ties & green marking	02/26/2020	26.52	.00	
Total 10-439-320:					26.52	.00	
<b>10-439-520</b>							
240	AVISTA UTILITIES	2180110000FE	ELECTRIC	02/19/2020	13.61	.00	
240	AVISTA UTILITIES	7852400000FE	ELECTRIC	02/19/2020	20.01	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
Total 10-439-520:					33.62	.00	
<b>10-439-630</b>							
136	AMERICA ON SITE SERVICES	329751	Handicap unit	03/02/2020	95.00	.00	
Total 10-439-630:					95.00	.00	
<b>10-440-429</b>							
251	BOCKELMAN, SUSAN JEAN	892	Seminar -Gardening	02/25/2020	72.00	.00	
Total 10-440-429:					72.00	.00	
<b>10-440-700</b>							
2761	SUB TERRA LLC	72275	Shirts	02/14/2020	118.60	.00	
2761	SUB TERRA LLC	72406	Shirts	02/21/2020	243.00	.00	
2761	SUB TERRA LLC	72407	Shirts	02/21/2020	276.75	.00	
Total 10-440-700:					638.35	.00	
<b>10-440-750</b>							
2815	VISA	ES1391FEB20	Feb credit card pymt MT	02/28/2020	41.27	.00	
Total 10-440-750:					41.27	.00	
<b>10-440-805</b>							
2815	VISA	ES1391FEB20	Feb credit card pymt ES	02/28/2020	199.12	.00	
Total 10-440-805:					199.12	.00	
<b>60-434-310</b>							
134	AMAZON CAPITAL SERVICES	13FL-H4L7-TM	Whiteboard,dry erase whiteboard,	03/05/2020	87.49	.00	
1240	J.THAYER COMPANY, INC	1424390-0	Envelopes, tape, binders, air dust	01/16/2020	37.44	.00	
2440	STAPLES ADVANTAGE	3439396991	hanging folders, legal folders	02/15/2020	29.90	.00	
2440	STAPLES ADVANTAGE	3439396992	Credit for inv 3439396992	02/13/2020	4.44	.00	
2518	SUPER ONE FOODS	07-2403173	Creamer	02/27/2020	4.67	.00	
2815	VISA	MT7773FEB20	Dec credit card pymt MT	01/28/2020	48.19	.00	
Total 60-434-310:					203.25	.00	
<b>60-434-421</b>							
161	ANDERSON BROS CPA'S PA	1381	Audit Progress Billing	02/29/2020	833.33	.00	
Total 60-434-421:					833.33	.00	
<b>60-434-510</b>							
75	TIME WARNER CABLE	002706402252	Internet fee	02/25/2020	132.50	.00	
2810	VERIZON WIRELESS, BELLEVU	9849157182	Cell phones -CH	02/25/2020	125.10	.00	
Total 60-434-510:					257.60	.00	
<b>60-435-520</b>							
240	AVISTA UTILITIES	0942300000FE	Water pumping	02/19/2020	277.96	.00	
240	AVISTA UTILITIES	1016020000FE	Water pumping	02/19/2020	888.21	.00	
240	AVISTA UTILITIES	3242300000FE	ELECTRIC	02/19/2020	44.75	.00	
240	AVISTA UTILITIES	3708160000FE	Water pumping	02/19/2020	329.18	.00	
240	AVISTA UTILITIES	4452300000FE	Water pumping	02/19/2020	113.25	.00	
1385	KOOTENAI ELECTRIC	1835508FEB20	Water pumping	02/28/2020	5,959.99	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
Total 60-435-520:					7,613.34	.00	
<b>60-436-320</b>							
103	ALSCO	LSPO2274056	Shop towels & medical supplies	02/17/2020	53.08	.00	
103	ALSCO	LSPO2276464	Shop towels & first aid supplies	02/24/2020	53.08	.00	
103	ALSCO	LSPO2278827	Shop towel & medical supplies	03/02/2020	53.08	.00	
891	H.D. FOWLER	I5395549	Rubber gasket	02/24/2020	21.00	.00	
910	Harbor Freight Tools	553913	4 drawer cabinet	03/02/2020	90.00	.00	
1650	NAPA AUTO STORE, CDA, INC.	897724	Brake pads, disc canister	02/25/2020	38.63	.00	
1650	NAPA AUTO STORE, CDA, INC.	898411	Gloves	03/03/2020	14.99	.00	
2144	RATHDRUM TRADING POST HA	5719/1	Aisle 16	02/25/2020	1.40	.00	
2328	SERIGHT'S ACE HARWARE 3	10692/3	Tire & tube	05/02/2019	21.59	.00	
2328	SERIGHT'S ACE HARWARE 3	11210/3BAL	Backpack sprayer	06/25/2019	40.49	.00	
2328	SERIGHT'S ACE HARWARE 3	12222/3	Auto cloths	01/08/2019	11.68	.00	
2328	SERIGHT'S ACE HARWARE 3	13039/3	Paint brusher & roller	12/31/2019	18.88	.00	
2328	SERIGHT'S ACE HARWARE 3	13689/3	Mouse traps & bait	02/25/2020	17.07	.00	
2328	SERIGHT'S ACE HARWARE 3	13774/3	Caulk	03/03/2020	14.01	.00	
Total 60-436-320:					448.98	.00	
<b>60-436-450</b>							
50	ACCURATE TESTING LABS,LLC	109641	Nitrate	03/03/2020	125.00	.00	
Total 60-436-450:					125.00	.00	
<b>60-436-480</b>							
1125	IDAHO STATE OCCUPATIONAL	BAT-21844 KIE	License renewal-Bain	02/28/2020	65.00	.00	
1125	IDAHO STATE OCCUPATIONAL	BAT-21845-202	Renewal Ap TJ McDowell	02/24/2020	30.00	.00	
1125	IDAHO STATE OCCUPATIONAL	DWD1-21691 K	License renewal-Kieffer	02/28/2020	65.00	.00	
Total 60-436-480:					160.00	.00	
<b>60-436-490</b>							
1120	IDAHO RURAL WATER ASSOCIA	628742	Certification Math Review	02/26/2020	120.00	.00	
Total 60-436-490:					120.00	.00	
<b>60-436-520</b>							
240	AVISTA UTILITIES	3179070000FE	ELECTRIC	02/19/2020	436.19	.00	
Total 60-436-520:					436.19	.00	
<b>60-490-429</b>							
1880	PASSWORD, INC.	10235793	Overcalls	03/02/2020	107.16	.00	
1880	PASSWORD, INC.	10235794	Admin fee	03/02/2020	3.00	.00	
Total 60-490-429:					110.16	.00	
<b>61-434-310</b>							
134	AMAZON CAPITAL SERVICES	13FL-H4L7-TM	Whiteboard, dry erase whiteboard	03/05/2020	87.49	.00	
1240	J.THAYER COMPANY, INC	1424390-0	Envelopes, tape, ribbon, air duster	01/16/2020	37.44	.00	
2440	STAPLES ADVANTAGE	3439396991	hangingfolders, legal folders	02/15/2020	29.90	.00	
2440	STAPLES ADVANTAGE	3439396992	Credit for inv 3439396992	02/13/2020	4.43	.00	
2518	SUPER ONE FOODS	07-2403173	Creamer	02/27/2020	4.67	.00	
2815	VISA	MT7773FEB20	Feb credit card charges MT	01/28/2020	48.20	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
Total 61-434-310:					203.27	.00	
<b>61-434-421</b>							
161	ANDERSON BROS CPA'S PA	1381	Audit Progress Billing	02/29/2020	833.34	.00	
Total 61-434-421:					833.34	.00	
<b>61-434-510</b>							
75	TIME WARNER CABLE	002706402252	Internet fee	02/25/2020	132.49	.00	
2810	VERIZON WIRELESS, BELLEVEU	9849157182	Celli phones -CH	02/25/2020	125.10	.00	
Total 61-434-510:					257.59	.00	
<b>61-435-520</b>							
240	AVISTA UTILITIES	3052400000FE	Sewer pumping	02/21/2020	7.57	.00	
240	AVISTA UTILITIES	5552300000FE	Sewer pumping	02/19/2020	227.88	.00	
240	AVISTA UTILITIES	6785020000FE	Sewer pumping	02/19/2020	1,144.06	.00	
240	AVISTA UTILITIES	7807930000FE	Sewer pumping	02/19/2020	28.54	.00	
Total 61-435-520:					1,408.05	.00	
<b>61-436-320</b>							
103	ALSCO	LSPO2274056	Shop towels & medical supplies	02/17/2020	53.07	.00	
103	ALSCO	LSPO2276464	Shop towels & medical supplies	02/24/2020	53.07	.00	
103	ALSCO	LSPO2278827	Shop towels & medical supplies	03/02/2020	53.07	.00	
910	Harbor Freight Tools	553913	4 drawer cabinet	03/02/2020	89.99	.00	
2328	SERIGHT'S ACE HARWARE 3	10909/3	Blade sawzal	05/23/2019	20.69	.00	
2328	SERIGHT'S ACE HARWARE 3	11210/3BAL	Backpack sprayer	06/25/2019	40.50	.00	
2328	SERIGHT'S ACE HARWARE 3	13719/3	Paint brush	02/27/2020	6.29	.00	
2328	SERIGHT'S ACE HARWARE 3	13778/3	Adapters & bushing	03/04/2020	6.97	.00	
Total 61-436-320:					323.65	.00	
<b>61-436-480</b>							
1125	IDAHO STATE OCCUPATIONAL	WWC1-21690	License renewal-Kieffer	02/28/2020	65.00	.00	
Total 61-436-480:					65.00	.00	
<b>61-436-520</b>							
240	AVISTA UTILITIES	3179070000FE	ELECTRIC	02/19/2020	436.19	.00	
Total 61-436-520:					436.19	.00	
<b>61-436-610</b>							
281	BIGFOOT TECHNICAL SERVICE	4995	Removed Avista's pole at Lift Stati	03/04/2020	1,249.86	.00	
Total 61-436-610:					1,249.86	.00	
<b>62-400-200</b>							
891	H.D. FOWLER	15399467	Meter adapters	02/27/2020	337.56	.00	
Total 62-400-200:					337.56	.00	
<b>66-400-400</b>							
281	BIGFOOT TECHNICAL SERVICE	4990	Radio system upgrade	03/03/2020	14,450.00	.00	

Vendor	Vendor Name	Invoice Number	Description	Invoice Date	Net Invoice Amount	Amount Paid	Date Paid
Total 66-400-400:					14,450.00	.00	
Grand Totals:					88,217.09	.00	

Dated: \_\_\_\_\_

Mayor: \_\_\_\_\_

City Council: \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

City Recorder: \_\_\_\_\_

Report Criteria:

Detail report.

Invoices with totals above \$0.00 included.

Only unpaid invoices included.



City of Rathdrum  
8047 W Main St Unit 1  
Rathdrum, ID 83858  
Connect Migration Budgetary Quote

Eric Zuleger  
Sr. Account Mgr.  
Black Box Network Services  
9155 Cottonwood Lane  
Maple Grove, MN 55369  
t.763-657-5439  
[www.blackbox.com](http://www.blackbox.com)

2/25/2020

This is a budgetary quote to migrate your phone system to Connect. This quote also includes a new ST50A switch to replace your existing unsupported switch. The City of Rathdrum is responsible for providing a server on a supported version of Hyper V. Web based Connect Client training, administration training and post cut support are included in this budgetary quote.

Item Qty	Product Part Number	Product Description
<b>Shoretel Hardware</b>		
1	SHO-10580	Mitel Voice Switch ST50A
<b>Partner Support</b>		
1	SHO-94111	Partner Support (1 Year, No Phones)(US Only)
<b>Implementation</b>		
1	Labor	Black Box Labor for Migration to Mitel MiVoice Connect
1	Labor	Project Management
1	Training	Connect Client Training by ComSolutions
1		<b>Shipping and Handling</b>
Total Hardware Price (including Shipping & Handling)		\$2,202.86
Total Software and/or Licensing Price		
Total Maintenance and/or Support Price		\$239.57
Total Professional Services Price		\$5,010.00
<b>Total Price</b>		<b>\$7,452.43</b>

The quotation provided herein by Black Box Network Services represents only a cursory view of this project and is strictly for budgetary purposes only and may be used as such for 60 days from the date of this quote. The final components, configuration, costs, performance and warranties for the actual completion of this project as represented in this budgetary quotation are subject to change at any time without prior notice. Furthermore, this budgetary quotation may not be comprehensive in nature, and may require additional charges, including but not limited to, engineering, design, hardware, software, licenses, permits, taxes, intellectual property, industrial property, testing, freight, patent rights or certification. Regardless of its content, this budgetary quote and all its contents are not intended to constitute a contract, binding agreement, or an amendment to any existing contract or agreement. This budgetary quotation and all of its contents contain Black Box Network Services information that is privileged, confidential, proprietary in nature, or subject to trademark and use or disclosure of this information without the prior written consent of Black Box Network Services is strictly prohibited



<https://www.visionms.net>

509-315-8845

P.O. Box 28429, Spokane, WA 99228

Quote VMSQ2678

Valid through March 20, 2020

**Prepared For:**

City of Rathdrum  
Leon Duce  
Phone: (208) 687-0261 x116  
8047 W. Main Street, #1  
Rathdrum, ID 83858  
leon@rathdrum.org

**Prepared By:**

Brandon Votaw  
Director of Information Technology  
Phone: 509-491-3960 x2  
Fax: 888-223-6007  
Email: bvotaw@visionms.net



For the full presentation proposal, [click here](#) to view or download the PDF version of this quote. You can sign and email the .pdf to [help@visionms.net](mailto:help@visionms.net), or you can save time by simply electronically accepting this quote below.

**Line Item Detail**

QTY	Description	Picture	Unit Price	Ext Price
1	Microsoft Server 2019 Standard - 16 Cores - 1 Physical Host License - 2 VM's - Local Government Pricing - Does Not include User CAL's		\$884.00	\$884.00

**SubTotal: \$884.00**  
**Shipping: \$0.00**  
**Sales Tax: \$53.04**  
**Total: \$937.04**

**Ready to Accept?**

**Order Confirmation**

We reserve the right to cancel orders arising from errors, inaccuracies, or omissions.

I agree to the terms and conditions of the above document and PDF attachment with an electronic signature below.

**IP Address** 74.87.233.98

**PO Number**

(Optional: Enter PO Number as your reference only.)

**Comments**

**Email**           leon@rathdrum.org

**Address**

**Printed Name**

**Signature**

*"signatures" could include: /john smith/; /js/; /js123/, etc*

**Have Questions?**

**Not Ready To Accept? Have Questions?**

*(Note, you will receive a copy of your message by email.)*

**No questions posted yet.**

*Time expressed in Pacific Standard Time UTC-08:00*

**PUBLIC WORKS DEPARTMENT**

8047 W. Main Street  
Rathdrum, ID 83858  
P 208.687.2700  
F 208.687.1377

# Memo

**To:** City Council  
**From:** Kevin Jump  
**CC:**  
**Date:** March 5, 2020  
**Re:** SH41 Widening Improvements

---

At the February 12<sup>th</sup> City Council meeting, we provided a brief presentation about ITD's financial participation request of the City of Rathdrum. We provided an itemized list of the financial components of ITD's request, the project schedule and a few matters which we are still researching/discussing.

The attached Cooperative Construction Agreement defines the expectations and obligations for the City of Rathdrum's participation in the SH41 Widening project.

The attached Memorandum of Understanding defines the maintenance and/or ownership responsibilities of certain elements which are being created as part of the SH41 Widening project.

## Financial Participation – by City of Rathdrum

The City of Rathdrum's Financial Participation items are identified on Exhibit A. These items were thoroughly discussed with ITD representatives and we find them to be accurate. However, there were a few lingering questions which were unanswered when we made our February 12<sup>th</sup> presentation.

- 1.) This is a federal aid project. With this said, ITD is receiving federal monies to construct the project. The ITD participation on this project is 7.34% (92.66% federal monies). For the certain major intersection improvements, on the SH41 corridor, the City of Rathdrum is being asked to financially participate. ITD has informed us that the City of Rathdrum is ineligible for participating at the local share-federal share financial terms. Thus, the financial participation terms, as presented to the City Council on February 12<sup>th</sup>, remain unchanged.

- 2.) To avoid expensive future borings under the widened SH41 pavement with either water and/or sanitary sewer extensions, we were looking to get utility casing pipes constructed as part of the project. As such, these utility casing pipe extensions are 100% Rathdrum's responsibility. The unanswered matters pertained to the direct benefit of property owners to develop their properties. Leon Duce has reached out to both Don Jacklin and Ragnar Arestad about their respective interests in financially participating in the identified utility casing pipe extensions.
- 3.) The payment schedule has not yet been determined. Although we remain hopeful that ITD will allow us to re-pay our project costs under a 10-year payment schedule, it is recognized that these terms have not yet been approved by the State of Idaho's financial controller. The attached agreement includes language, under Section II, 2., provides for the ability to negotiate a mutually-agreement payment schedule.

#### Memorandum of Understanding

The attached memorandum of understanding defines certain maintenance obligations for both the City of Rathdrum and ITD. These items were thoroughly discussed with ITD representatives.

#### Requested Action of Rathdrum City Council

We are herein seeking that the Rathdrum City Council approve the attached Cooperative Construction Agreement and Memorandum of Understanding and authorize Mayor Holmes to execute both agreements on behalf of the City of Rathdrum.



**IDAHO TRANSPORTATION DEPARTMENT**

600 W. Prairie Ave.  
Coeur d'Alene, ID 83815-8764

(208) 772-1200  
itd.idaho.gov

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March 3, 2020

Mr. Kevin Jump, P.E.  
Public Works Director  
City of Rathdrum  
8047 Main St #1  
Rathdrum, ID 83858

**Projects: SH-41, E PRAIRIE AVE TO LANCASTER RD, KOOTENAI CO  
Project No. A020(098); Key No. 20098**

**SH-41, LANCASTER RD TO BOEKEL RD, RATHDRUM  
Project No. A020(120); Key No. 20120**

**SUBJECT: Cooperative Construction Agreement & Memorandum of Understanding Signatures  
Required**

Dear Kevin or whomever it may concern:

It is the Idaho Transportation Departments (ITD) understanding that the above mentioned projects Cooperative Construction Agreement and the Memorandum of Understanding between ITD and the City of Rathdrum (CITY) will be presented to the city council on March 11, 2020.

ITD is expecting a bid opening of the project on March 10<sup>th</sup>, 2020. A qualifying low bid up to the Engineer's Estimate plus 10%, ITD will award the project to the apparent low bidder within 15 days. This award is contingent on the agreements with the City of Rathdrum and the City of Post Falls being fully executed prior to award.

It is with utmost importance that the Cooperative Agreement and Memorandum of Understanding between ITD and the City be approved and signed by the City at the meeting on March 11<sup>th</sup>, 2020.

If you have any questions regarding this letter, please contact Chancellor Cole, ITD Project Manager, at 208-772-8021.

Sincerely,

A handwritten signature in blue ink, appearing to read "Ryan Hawkins".

Ryan Hawkins, P.E.  
Design Construction Engineer A

---

**COOPERATIVE CONSTRUCTION AGREEMENT**  
**For the Construction of:**

**SH41, PRAIRIE AVE TO LANCASTER RD**  
**PROJECT NO. A020(098)**  
**KEY NO. 20098**

**SH41, LANCASTER RD TO BOEKEL RD**  
**PROJECT NO. A020(120)**  
**KEY NO. 20120**

THIS AGREEMENT is made and entered into this \_\_\_\_\_ day of \_\_\_\_\_, \_\_\_\_\_, by and between the **IDAHO TRANSPORTATION DEPARTMENT**, hereafter called the State, and the **CITY OF RATHDRUM** hereafter called the City. This agreement will terminate with the final acceptance of the construction project in accordance with Subsection 105.15 of the 2018 Standard Specifications for Highway Construction.

**AGREEMENT:**

This agreement shall supersede previous construction agreements for SH41 within the stated limits but shall be complimentary to the Memorandum of Understanding for Maintenance dated \_\_\_\_\_.

**PURPOSE:**

The State has scheduled projects to reconstruct SH41 from Prairie Avenue to Lancaster Road and from Lancaster Road to Boekel Road. Work on the projects includes roadway and bridge construction, including traffic signals, utility work, bicycle & pedestrian facilities, and a pedestrian tunnel in the UPRR mainline overpass north embankment (MP 4.06). This Agreement will provide for the responsibilities of the parties regarding construction and maintenance of the projects.

Authority for this Agreement is established by Section 40-317, Idaho Code.

The parties agree as follows:

**SECTION I** - The State will:

1. Program the construction of the project and execute all necessary agreements with the Federal Highway Administration, securing the Federal Government's pro rata participation in the eligible construction costs.
2. Advertise for the construction of the projects, open bids, prepare a contract estimate of cost based on the successful low bid, and notify the City thereof.
3. Award contracts for construction of the separate projects based on the successful low bids.
4. Assume its share of all construction and engineering costs for the projects, including but not limited to road and bridge construction, traffic signal installation, illumination, combined use pedestrian/bicycle pathway, utilities, and miscellaneous specialty items necessary to complete the projects. City's financial responsibilities for the projects are detailed in Exhibit A.

5. Construct the following items and utilities solely at the financial responsibility of the City. The City will reimburse the State 100% of the actual design and construction cost of these items. Estimated costs for these items are detailed on Exhibit A:
  - a. Reset Fire Hydrants that do not currently have an existing easement.
  - b. City water line & sewer line that do not currently have an existing easement.
  - c. Conduit sleeves for future expansion of City utilities.
  - d. Conduit sleeves to accommodate the installation of irrigation to the swales and between Hayden and Lancaster for irrigation to the median swales.
6. Designate a resident engineer and other personnel, as the State deems necessary, to supervise and inspect construction of the projects in accordance with the plans, specifications and estimates in the manner required by applicable state and federal regulations. This engineer, or his authorized representatives, will prepare all monthly and final contract estimates and change orders.
7. In cooperation with the City, establish and cause to be maintained all detours deemed necessary to best serve the public interests and to expedite the work.
8. Install initial placement of Special Pavement Markings – stop bars, crosswalks, and arrows. Furnish and install all official guide signs at junctions of the urban extensions to the state highway system and all confirming and reassurance route markers and guide arrows along the urban extensions of the state highway system necessary to properly identify the State highways.
9. Through issuance of an Encroachment Permit, allow the City to retain, maintain, connect to and improve all existing City-owned water lines, storm sewers, and sanitary sewer now in place on the state highway right-of-way and also new facilities to be installed within the new additional right-of-way.
10. Maintain complete accounts of all project funds received and disbursed, which accounting will determine the final project costs.
11. Upon completion of the projects, after all costs have been accumulated and the final voucher paid by the Federal Highway Administration, provide a statement to the City summarizing the estimated and actual costs, indicating an adjustment for or against the City. Any excess funds transmitted by the City and not required for the projects will be returned.
12. Retain ownership of the controller and cabinet for the traffic signals to be installed and assume all necessary maintenance responsibilities and costs thereof, except as provided in Sections II and III of this agreement, that will be required to keep the traffic signal, as installed, in continuous operation in conformance with the requirements of the Manual on Uniform Traffic Control Devices for Streets and Highways, as adopted by the State.
13. Upon completion of construction and annually thereafter, perform a complete check of the traffic signal equipment and operation, replace all signal lamps on a scheduled basis.
14. Provide to the City of Rathdrum as-built plans of the completed project, in hard copy (11x17 & 22x34) and digital (.pdf and .dwg or .dgn) including surveyed locations (Idaho State Plane NAD 83 1992 adjustment Horizontal and NAVD 1988 vertical) of Sewer cleanouts, sewer manholes,

termini of sewer mains, drywells, hydrants, horizontal and vertical fittings, valves, conduits for casings (City Utility), and termini of sewer services installed by the project.

15. Provide at City expense a 4" conduit for future City of Rathdrum with pull boxes spacing not to exceed 1,000 feet under pathways and to center medians.
16. Allow for the City to provide pedestrian scale illumination for pedestrian trails along SH41 subject to appropriate coordination thru issuance of encroachment permits by ITD (see also section 2.16 (h))

**SECTION II** - The City will:

1. Hereby approve the plans and specifications for bid.
2. Within sixty (60) days of the first day of January 4, 2021, begin paying the State the actual sum represented by their share of work approved and awarded, estimated to be \$1,718,632.47, via a mutually agreeable payment schedule with all remaining balance due on or before final acceptance of the project.
3. Remit funds owed the State through the ITD payment portal at:  
<https://apps.itd.idaho.gov/PayITD> .
4. Upon receipt of notification by the State of the lowest qualified bid received, if the City's share does not exceed the amount set forth in Section II, Paragraph 2, by ten percent then transmit to the State the City's share of such excess cost or enter into an agreement to pay such excess cost on a mutually agreeable payment schedule.
5. Upon receipt of notification by the State for the water and sewer line, approve or deny bids for the respective items.
6. Upon receipt of the statement referred to in Section I, Paragraph 12, indicating an adjustment in cost against the City, promptly remit to the State that amount through the ITD Payment Portal.
7. Cooperate with the State in the selection and designation of suitable detour(s) routing during project construction.
8. Through appropriate ordinance and police power, cooperate with and assist the State in prohibiting and removing encroachments on any part of the state highway right-of-way within the project limits as defined under this Agreement.
9. Require that access points be shared whenever possible and will encourage the connection of parking areas during site plan review to encourage off-corridor circulation by permitting the movement of vehicles from property to property. The City may assist in determination of such access patterns by requiring additional considerations to facilitate off-corridor circulation.
10. Will encourage the development of collector or connector streets parallel to Highway 41 to promote the flow of traffic to controlled intersections.

11. Upon request to energize, assume all energy costs required to operate the traffic signals. Promptly notify the State of any equipment malfunction or required maintenance as outlined in the Memorandum of Understanding between the State and the City.
12. Obtain concurrence of the State before using the traffic signal poles or mast arms for any purpose other than to support traffic control devices or luminaires.
13. Maintain, erect, or install within the project limits only those traffic control devices, including signs, that are in conformance with the Manual of Uniform Traffic control Devices for Streets and Highways, as adopted by the State.
14. Upon completion of the project, accept from the State ownership of all highway and street lighting equipment to be installed within the projects limits north of Hayden Avenue minus the intersection, and upon request to energize, assume all operation, and the costs thereof, required to maintain the equipment in continuous service during the hours of darkness, and not remove, alter or abandon the lighting equipment without the prior concurrence of the State. In the event that the "Average Maintained Foot- Candles" of the lighting system drops below seventy (70) percent of the original installed values, the City shall cause the light intensity to be restored to approximately the original values. Any combination of re-lamping and/or cleaning of fixtures may be used to achieve the necessary desired intensity.
15. Comply with all pertinent sections of the current ITD Standard Specifications for Highway Construction in accomplishing all future trench backfill and pavement repairs on the state highways within the project limits.
16. Apply for an Encroachment Permit from the State before installing or constructing any new, or relocating, any existing bicycle and pedestrian facilities or any existing City-owned water line, storm sewer, sanitary sewer or other facilities on the state highways within the project limits.
17. Obtain concurrence from the State before vacating or closing any access points connecting to the state highways within the project limits.
18. Obtain approval from the State before accepting any new street or alley access points connecting to the state highways within the project limits.

**SECTION III** – General:

1. The Traffic Signal at Hayden Avenue & SH-41 and any Signal south of this intersection are not included in this agreement.
2. Should any of the traffic signal installation be damaged or destroyed through the wrongful or negligent act of any third party, the City and State will make every effort to determine the identity and whereabouts of the responsible party, and the State will attempt collection of the cost of repair or replacement. The State agrees to advance funds for the repair or replacement based upon their proportionate share of the cost.
3. Sufficient Appropriation. It is understood and agreed that the State and City are governmental

agencies, and this Agreement shall in no way be construed so as to bind or obligate either beyond the term of any particular appropriation of funds by the Federal Government, the State Legislature or City Council as may exist from time to time. The State and the City reserve the right to terminate this Agreement if, in its sole judgment, the Federal Government, the legislature of the State of Idaho or the City Council fails, neglects or refuses to appropriate sufficient funds as may be required for the State or the City to continue payments. Any such termination shall take effect immediately upon notice and be otherwise effective as provided in this Agreement.

4. This Agreement shall become effective on the first date mentioned above and shall remain in full force and effect until amended or replaced upon the mutual consent of the State and the City.
5. Subject to the limitations hereinafter set forth, each Party shall be responsible only for the acts, omissions or negligence of such Party's own actions. Nothing in the Contract shall extend the tort responsibility or liability of either Party beyond that required by the Idaho Tort Claims Act, Idaho Code § 6-901 et seq. Each Party shall be responsible for damages to other parties in the performance of the Agreement.

**EXECUTION:**

This Agreement is executed for the State by its District Engineer for District One, and executed for the City by the Mayor, attested to by the City Clerk, with the imprinted Corporate Seal of the City of Rathdrum.

**IDAHO TRANSPORTATION DEPARTMENT**

\_\_\_\_\_  
District Engineer

**ATTEST:**

**CITY OF RATHDRUM**

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Mayor

(SEAL)

By regular/special meeting  
on \_\_\_\_\_.

gfb: 20098 & 20120 Rathdrum Const Coop.docx

**A020(098) SH-41, E PRAIRIE AVE TO LANCASTER RD, KOOTENAI CO - KN 20098**  
**A020(120), SH-41, LANCASTER RD TO BOEKEL RD, RATHDRUM - KN 20120**

**EXHIBIT "A"**

1. City financial obligation for traffic signals (pro-rated based on number of additional signalized lanes entering the intersection from City Streets).
    - a. Lancaster Road & SH 41: (City = 25% (4 of 16), State = 75%)
      - Estimated Total Cost = \$ 700,000
      - Lancaster Road – 8 lanes (4 lanes from the west)
      - SH 41 – 8 lanes (4 lanes from each direction, north & south)
        - City Portion = \$ **175,000.00**
        - State Portion = \$ 525,000.00
    - b. Nagel Road & SH 41: (City = 42.9% (6 of 14), ITD = 57.1%)
      - Estimated Total Cost = \$ 600,000
      - Nagel Ave – 6 lanes (3 lanes from each direction, east & west)
      - SH 41 - 8 lanes (4 lanes from each direction, north & south)
        - City Portion = \$ **257,400.00**
        - State Portion = \$ 342,600.00
  2. Utility work - City of Rathdrum owned:  
 See **Exhibit A-1** - Part 1-3 of 3
    - Water and Sewer Line Design and Construction
      - Design Costs estimated at \$24,313.74. **Exhibit A-1 Part 1 of 3**
      - Construction Costs estimated at \$944,994.72 - **Exhibit A-1 Part 2 of 3**
      - City Cost = \$881,968.73 - **Exhibit A-1 Part 3 of 3**
    - Sleeves to accommodate the installation of irrigation to the swales.
      - 3 median swales (2 ea 4” sleeves x 50 ft x 3 locations = 300 Ft estimated at \$7.00 per Ft
      - City Cost = \$2,100.00
      - 19 crossings to sidewalk swales (2” sleeves x 30 ft) – 570 Ft estimated at \$5.00 per Ft
      - City Cost = \$ 2,850.00
2. Subtotal = **\$ 911,232.47**

3. UPRR Mainline and SH-41 Grade Separation:
  - State will provide additional 12’ width with separation barrier on each side of bridge and a pedestrian/bike tunnel in the north embankment for future pedestrian/bike accommodations.
  - State will provide routine maintenance for the surface of the bridge, within the limits of the separation barrier. northbound and southbound separation barrier.
  - City will provide routine maintenance, snow removal, sweeping and graffiti removal for anything outside of the outside of the main structure and approaches.
    - City will be responsible for 25% of the cost of the west side widening of the main structure and approaches.
    - Estimated construction cost for widened bridge and approaches and 10% for mobilization = \$1,200,000.00.
    - 10% for revised design and 15% for construction engineering & contingency = \$300,000.00.
    - Total estimated cost = \$1,500,000.00
    - City portion = \$ **375,000.00**
4. UPRR North Underpass
  - State will provide an underpass in the northern fill of the UPRR Mainline and SH-41 Grade Separation to accommodate future east-west pedestrian and bicycle movement.
  - City will provide routine maintenance, snow removal, sweeping and graffiti removal for the underpass in addition to routine maintenance of the shared use path.

<u>Total City financial responsibility =</u>	1 (a)	\$ 175,000.00
	1(b)	\$ 257,400.00
	2	\$ 911,232.47
	3	\$ 375,000.00
		<u><b>\$ 1,718,632.47</b></u>



**SH 41, Lancaster Rd to Boekel Rd, Rathdrum  
Key No. 20120  
Agreement No. 4 - City of Rathdrum Portion**

December 11, 2019

<b>A1</b>	<b>T-O Labor</b>			
	Staff	Hours	Rate	Labor Cost
1	Senior Engineer	64	\$52.74	\$3,375.36
2	Engineer	25	\$44.33	\$1,108.25
3	Survey Manager	0	\$46.97	\$0.00
4	Surveyor	0	\$33.00	\$0.00
5	Survey Assist./EI	171	\$27.27	\$4,663.17
6	Admin	3	\$22.59	\$67.78
	<b>Sub Total</b>	<b>263</b>		<b>\$9,214.56</b>
<b>B1</b>	<b>T-O Payroll, Fringe Benefit Costs &amp; Overhead</b>			
	Total Raw Labor Cost		Approved Rate	Overhead Total
	\$9,214.56		124.03%	<b>\$11,428.81</b>
<b>C1</b>	<b>Net Fee</b>			
	Total Raw Labor & Overhead		Net Fee	Fee Total
	\$20,643.37		12%	<b>\$2,477.20</b>
<b>D1</b>	<b>FCCM</b>			
	Total Raw Labor Cost		Approved FCCM Rate	FCCM Total
	\$9,214.56		0.58%	<b>\$53.44</b>
			<b>T-O Labor Total</b>	<b>\$23,174.02</b>
<b>E1</b>	<b>Escalation</b>			
	Total Raw Labor & Overhead Esc		Ratio Annual Esc	
	\$20,289.53		0% <b>3.00%</b>	<b>\$0.00</b>
<b>E2</b>	<b>Expenses</b>			
	Item	Estimated Amount	Unit Cost	Estimated Expense
1	Mileage	34	\$0.580	\$19.72
2	Postage	\$0	n/a	\$0.00
3	Printing	\$0	n/a	\$0.00
4	CADD Hours	112	\$10.00	\$1,120.00
5	GPS/Robot Hours	0	\$65.00	\$0.00
6	Agency Fees/Title Reports	\$0	n/a	\$0.00
7	Monument Stamping	\$0	n/a	\$0.00
			<b>T-O Expenses Total</b>	<b>\$1,139.72</b>
			<b>T-O Total</b>	<b>\$24,313.74</b>



SH 41, Lancaster Rd to Boekel Rd, Rathdrum

Key No. 20120

Agreement No. 4



		T-O Engineers						
		Senior Engineer	Engineer	Survey Manager	Surveyor	Survey Assist./EI	Admin	Total
<b>1.0 ADMINISTRATION</b>								
1.1	Team Progress Meetings							0
1.2	Progress Billing							0
<b>Subtotal Administration</b>		0	0	0	0	0	0	0
<b>2.0 SURVEY &amp; MAPPING</b>								
2.1	Supplemental Field Utility Survey							
	Field Survey Work							0
	Data Processing and Checking							0
<b>Subtotal Survey and Mapping</b>		0	0	0	0	0	0	0
<b>3.0 Utilites</b>								
3.1	City of Rathdrum Water and Sewer							
3.1.1	Water and Sewer Coordination	10	4			7		21
3.1.2	Water and Sewer Plans and Details (13 sheets)	34	14			130		178
3.2.3	Water and Sewer Special Provisions	10	3			21	3	37
3.1.4	Water and Sewer Quantities and Estimate	5	4			10		19
3.1.5	Water and Sewer Final Submittal and QLPE Review	5				3		8
3.2	Utility Coordination							
	Utility Coordination							0
3.3	Parcel 33 Septic Relocation Coordination							
	Parcel 33 Septic Relocation Coordination							0
<b>Subtotal Utilites</b>		64	25	0	0	171	3	263
<b>Grand Total</b>		64	25	0	0	171	3	263



SH 41, Lancaster Rd to Boekel Rd, Rathdrum

Key No. 20120

Agreement No. 4



		T-O Engineers						
		Mileage	Postage	Printing	CADD Hours	GPS/Robot Hours	Agency Fees/Title Reports	Monument Stamping
<b>1.0</b>	<b>ADMINISTRATION</b>							
1.1	Team Progress Meetings							
1.2	Progress Billing							
	<b>Subtotal Administration</b>	0	0	0	0	0	0	0
<b>2.0</b>	<b>SURVEY &amp; MAPPING</b>							
2.1	Supplemental Field Utility Survey							
	Field Survey Work							
	Data Processing and Checking							
	<b>Subtotal Survey and Mapping</b>	0	0	0	0	0	0	0
<b>3.0</b>	<b>Utilites</b>							
3.1	City of Rathdrum Water and Sewer							
3.1.1	Water and Sewer Coordination	17						
3.1.2	Water and Sewer Plans and Details				112			
3.2.3	Water and Sewer Special Provisions							
3.1.4	Water and Sewer Quantities and Estimate							
3.1.5	Water and Sewer Final Submittal and QLPE Review	17						
3.2	Utility Coordination							
	Utility Coordination							
3.3	Parcel 33 Septic Relocation Coordination							
	Parcel 33 Septic Relocation Coordination							
	<b>Subtotal Utilites</b>	34	0	0	112	0	0	0
	<b>Grand Total</b>	34	0	0	112	0	0	0

Total Cost

Estimate

Estimated Cost: \$899,994.97

Contingency: 5.00%

**Estimated Total: \$944,994.72**

Base Date: 12/11/19

Spec Year: 01

Unit System: E

Work Type: RECONSTRUCTION/REALIGNMENT

Highway Type: Four Lane

Urban/Rural Type: RURAL

Season: SUMMER

County: Kootenai

Latitude of Midpoint: 0

Longitude of Midpoint: 0

District: 18112

Federal Project Number: A020(098)

State Project Number:

*Prepared by J Allen*

Estimate:

Idaho Transportation Department

<u>Item Number</u> <u>Description</u> <u>Supplemental Description</u>	<u>Quantity</u>	<u>Units</u>	<u>Unit Price</u>	<u>Extension</u>
<b>203-125C</b> REM OF MISCELLANEOUS ITEMS	1,136.00	FT	\$10.00000	\$11,360.00
<b>605-640A</b> ADJUST VALVE COVERS	7.00	EACH	\$700.00000	\$4,900.00
<b>S600-20A</b> FIRE HYDRANT ASSEMBLY	3.00	EACH	\$5,500.00000	\$16,500.00
<b>S600-30A</b> PIPE FITTING (12" PIPE COUPLING)	2.00	EACH	\$1,000.00000	\$2,000.00
<b>S600-30B</b> PIPE FITTING (18" PIPE COUPLING)	1.00	EACH	\$1,000.00000	\$1,000.00
<b>S600-30C</b> PIPE FITTING (12" 11.25 DEG ELBOW)	2.00	EACH	\$800.00000	\$1,600.00
<b>S600-30D</b> PIPE FITTING (18" 22.5 DEG ELBOW)	4.00	EACH	\$1,000.00000	\$4,000.00
<b>S600-30E</b> PIPE FITTING (18" 11.25 DEG ELBOW)	6.00	EACH	\$1,000.00000	\$6,000.00
<b>S600-30F</b> PIPE FITTING (12" TEE)	1.00	EACH	\$800.00000	\$800.00
<b>S600-30G</b> PIPE FITTING (12"x12"x10" TEE)	1.00	EACH	\$1,050.00000	\$1,050.00

4:19:30PM

Page 2 of 4

Wednesday, December 11, 2019

Estimate:

Idaho Transportation Department

<u>Item Number</u> <u>Description</u> <u>Supplemental Description</u>	<u>Quantity</u>	<u>Units</u>	<u>Unit Price</u>	<u>Extension</u>
<b>S600-30H</b> PIPE FITTING (12"x12"x6" TEE)	3.00	EACH	\$1,000.00000	\$3,000.00
<b>S600-30I</b> PIPE FITTING (18"x18"x6" TEE)	1.00	EACH	\$1,200.00000	\$1,200.00
<b>S600-30J</b> PIPE FITTING SP CASING SPACER SUPPORT	37.00	EACH	\$125.00000	\$4,625.00
<b>S600-30K</b> PIPE FITTING (18" CROSS)	1.00	EACH	\$3,000.00000	\$3,000.00
<b>S600-30L</b> PIPE FITTING (18" X 12" REDUCER)	2.00	EACH	\$800.00000	\$1,600.00
<b>S900-50B</b> CONTINGENCY AMOUNT POTHOLE GAS LINE	1.00	CA	\$1,000.00000	\$1,000.00
<b>S901-05A</b> SP 10" GATE VALVE	1.00	EACH	\$2,400.00000	\$2,400.00
<b>S901-05B</b> SP 12" GATE VALVE	6.00	EACH	\$2,800.00000	\$16,800.00
<b>S901-05H</b> SP 18" BUTTERFLY VALVE	3.00	EACH	\$3,750.00000	\$11,250.00
<b>S901-05Z</b> SP FIRE HYDRANT RELOCATION	1.00	EACH	\$2,750.00000	\$2,750.00

4:19:30PM

Page 3 of 4

Wednesday, December 11, 2019

Estimate:

Idaho Transportation Department

<u>Item Number</u> <u>Description</u> <u>Supplemental Description</u>	<u>Quantity</u>	<u>Units</u>	<u>Unit Price</u>	<u>Extension</u>
<b>S901-06U</b> SP SAND BAGS	115.00	EACH	\$30.00000	\$3,450.00
<b>S911-05A</b> SP 10" WATER MAIN	302.00	FT	\$45.00000	\$13,590.00
<b>S911-05C</b> SP 18" WATER MAIN	1,113.00	FT	\$55.00000	\$61,215.00
<b>S911-05G</b> SP 24" UTILITY CASING	256.00	FT	\$180.00000	\$46,080.00
<b>S911-05I</b> SP 36" UTILITY CASING	503.00	FT	\$425.00000	\$213,775.00
<b>S911-05J</b> SP PIPE INSULATION	187.00	FT	\$50.00000	\$9,350.00
<b>S911-05K</b> SP 12" WATER MAIN	1,969.00	FT	\$50.00000	\$98,450.00
<b>S911-05L</b> SP 28" UTILITY CASING	988.00	FT	\$320.00000	\$316,160.00
<b>Z629-05A</b> MOBILIZATION	1.00	LS	\$41,089.97000	\$41,089.97
<b>Estimate Total</b>				<b>\$899,994.97</b>

4:19:30PM

Page 4 of 4

Wednesday, December 11, 2019

City of Rathdrum  
Portion

Estimate

Estimated Cost: \$839,970.22

Contingency: 5.00%

**Estimated Total: \$881,968.73**

Base Date: 12/11/19

Spec Year: 01

Unit System: E

Work Type: RECONSTRUCTION/REALIGNMENT

Highway Type: Four Lane

Urban/Rural Type: RURAL

Season: SUMMER

County: Kootenai

Latitude of Midpoint: 0

Longitude of Midpoint: 0

District: 18112

Federal Project Number: A020(098)

State Project Number:

*Prepared by J Allen*

Estimate:

Idaho Transportation Department

<u>Item Number</u> <u>Description</u> <u>Supplemental Description</u>	<u>Quantity</u>	<u>Units</u>	<u>Unit Price</u>	<u>Extension</u>
<b>203-125C</b> REM OF MISCELLANEOUS ITEMS	721.00	FT	\$10.00000	\$7,210.00
<b>605-640A</b> ADJUST VALVE COVERS	5.00	EACH	\$700.00000	\$3,500.00
<b>S600-20A</b> FIRE HYDRANT ASSEMBLY	3.00	EACH	\$5,500.00000	\$16,500.00
<b>S600-30A</b> PIPE FITTING (12" PIPE COUPLING)	2.00	EACH	\$1,000.00000	\$2,000.00
<b>S600-30B</b> PIPE FITTING (18" PIPE COUPLING)	1.00	EACH	\$1,000.00000	\$1,000.00
<b>S600-30C</b> PIPE FITTING (12" 11.25 DEG ELBOW)	2.00	EACH	\$800.00000	\$1,600.00
<b>S600-30D</b> PIPE FITTING (18" 22.5 DEG ELBOW)	1.00	EACH	\$1,000.00000	\$1,000.00
<b>S600-30E</b> PIPE FITTING (18" 11.25 DEG ELBOW)	0.00	EACH		\$0.00
<b>S600-30F</b> PIPE FITTING (12" TEE)	1.00	EACH	\$800.00000	\$800.00
<b>S600-30G</b> PIPE FITTING (12"x12"x10" TEE)	1.00	EACH	\$1,050.00000	\$1,050.00

4:18:24PM

Page 2 of 4

Wednesday, December 11, 2019

Estimate:

Idaho Transportation Department

<u>Item Number</u>	<u>Quantity</u>	<u>Units</u>	<u>Unit Price</u>	<u>Extension</u>
<u>Description</u>				
<u>Supplemental Description</u>				
<b>S600-30H</b>	3.00	EACH	\$1,000.00000	\$3,000.00
PIPE FITTING (12"x12"x6" TEE)				
<b>S600-30I</b>	0.00	EACH		\$0.00
PIPE FITTING (18"x18"x6" TEE)				
<b>S600-30J</b>	37.00	EACH	\$125.00000	\$4,625.00
PIPE FITTING SP CASING SPACER SUPPORT				
<b>S600-30K</b>	0.00	EACH		\$0.00
PIPE FITTING (18" CROSS)				
<b>S600-30L</b>	2.00	EACH	\$800.00000	\$1,600.00
PIPE FITTING (18" X 12" REDUCER)				
<b>S900-50B</b>	1.00	CA	\$1,000.00000	\$1,000.00
CONTINGENCY AMOUNT POTHOLE GAS LINE				
<b>S901-05A</b>	1.00	EACH	\$2,400.00000	\$2,400.00
SP 10" GATE VALVE				
<b>S901-05B</b>	6.00	EACH	\$2,800.00000	\$16,800.00
SP 12" GATE VALVE				
<b>S901-05H</b>	0.00	EACH		\$0.00
SP 18" BUTTERFLY VALVE				
<b>S901-05Z</b>	0.00	EACH		\$0.00
SP SP FIRE HYDRANT RELOCATION				

Estimate:

Idaho Transportation Department

<u>Item Number</u>	<u>Quantity</u>	<u>Units</u>	<u>Unit Price</u>	<u>Extension</u>
<u>Description</u>				
<u>Supplemental Description</u>				
<b>S901-06U</b>	115.00	EACH	\$30.00000	\$3,450.00
SP SAND BAGS				
<b>S911-05A</b>	302.00	FT	\$45.00000	\$13,590.00
SP 10" WATER MAIN				
<b>S911-05C</b>	678.00	FT	\$55.00000	\$37,290.00
SP 18" WATER MAIN				
<b>S911-05G</b>	256.00	FT	\$180.00000	\$46,080.00
SP 24" UTILITY CASING				
<b>S911-05I</b>	503.00	FT	\$425.00000	\$213,775.00
SP 36" UTILITY CASING				
<b>S911-05J</b>	187.00	FT	\$50.00000	\$9,350.00
SP PIPE INSULATION				
<b>S911-05K</b>	1,969.00	FT	\$50.00000	\$98,450.00
SP 12" WATER MAIN				
<b>S911-05L</b>	988.00	FT	\$320.00000	\$316,160.00
SP 28" UTILITY CASING				
<b>Z629-05A</b>	1.00	LS	\$37,740.22000	\$37,740.22
MOBILIZATION				
<b>Estimate Total</b>				<b>\$839,970.22</b>



**MEMORANDUM OF UNDERSTANDING**  
**Between**  
**THE IDAHO TRANSPORTATION DEPARTMENT**  
**And**  
**THE CITY OF RATHDRUM**

**PURPOSE**

The IDAHO Transportation Department (ITD or the State) and the City of Rathdrum (the City) desire to provide for the maintenance of state highway routes for SH 41 M.P. 3.450 (North R/W line of Hayden Ave.) to M.P. 6.460 (Boekel Rd.) (hereinafter referred to as the Project Section) and to arrange herein for the particular maintenance functions to be performed by the City and those to be performed by the State and to specify the terms and conditions under which such work will be performed.

**AUTHORITY**

Authority for this Memorandum of Understanding is established by Sections 40-310(5), 40-317(4), 67-2326 through 67-2333 of the Idaho Code.

**RESPONSIBILITIES AND PROCEDURES**

This Memorandum of Agreement supersedes previous Cooperative Maintenance Agreements for the Project Section, but shall be complimentary to the Cooperative Construction Agreement dated \_\_\_\_\_ until such time of completion of the project and upon all assumption and responsibilities by the Parties as noted within said Cooperative Construction Agreement. In consideration of the mutual covenants and premises herein contained, it is agreed that the City will perform such maintenance work as is specifically delegated to it and the State will perform those particular functions of maintenance delegated to it on the state highway routes or portions thereof as hereinafter described under Sections 13 and 14 hereof, or as said sections may be subsequently modified with the written consent of the parties hereto acting by and through their authorized representatives.

**1. MAINTENANCE DEFINED**

Maintenance is defined as follows:

- a. The preservation and keeping of right-of-way and each type of roadway, structure, and facility in the safe and usable condition to which it has been improved or constructed, but does not include reconstruction or other improvement.
- b. Provisions as necessary for the safety and convenience of traffic and the upkeep of traffic control devices.
- c. General utility services such as roadside planting and vegetation control.
- d. Special or emergency maintenance or repair necessitated by accidents or by storms or other weather conditions, slides, settlements, or other unusual or unexpected damage to a roadway, structure or facility.
- e. Upkeep of illumination fixtures on the streets, roads, highways, and bridges, which are required for the safety of persons using the said streets, roads, highways, and bridges.

**2. DEGREE OF MAINTENANCE**

The degree and type of maintenance for each highway or portion thereof shall mean doing the work and furnishing the materials and equipment to maintain the highway facility herein described in a manner as near as practicable to the standard in which they were originally constructed and subsequently improved.

### **3. HIGHWAY**

Highway, as used herein, includes the entire right-of-way in the Project Section which is secured or reserved for use in the construction and maintenance of the traveled way and roadsides as hereinafter described.

### **4. ROADWAY**

Roadway means the area between the inside face of curbs or the area between the flow lines of paved gutters; otherwise, the entire width within the highway which is improved for vehicular use including improved shoulders and side slopes, if they exist.

### **5. IMPROVED ROADSIDES**

Improved roadside is the area between the roadway, as defined under Section 4, and the right-of-way boundary lines, including curb and bicycle and pedestrian facilities.

Curb relates to a timber, concrete, asphalt, or masonry structure separating or otherwise delineating the roadway from the remainder of the highway and shall include paved gutters. A bicycle and pedestrian facility applies to the paved or otherwise improved surface area between the face of curb or edge of roadway and right-of-way boundary, including sidewalks, shared use paths, and other facilities as described in the attached Administrative Policy A-28-04. Paved entrances and driveways are considered part of the improved roadside.

### **6. UNIMPROVED ROADSIDES**

Unimproved roadsides relate to the area between the roadway and right-of-way boundary wherein curbs and pedestrian specific facilities do not exist.

### **7. BRIDGES**

Bridges are structures that span more than 20 feet measured between abutments along the centerline of the street and multiple span structures where the individual spans are in excess of 10 feet measured from center-to-center of supports along the centerline of the street. All other cross-drainage structures shall be classified as culverts.

### **8. TRAFFIC CONTROL DEVICES**

Traffic control devices include all signs, pavement markings, and highway illumination placed on or adjacent to the street or highway for the regulations, guidance, warning and aid of pedestrian and traffic movement thereon.

### **9. FRONTAGE ROADS**

Frontage roads are roads constructed on either side of the highway to provide authorized road access to adjacent properties in lieu of access directly from the highway.

### **10. ROUTINE MAINTENANCE**

Routine maintenance to be performed on the roadway or roadsides shall consist of such work as patching, spot sealing, crack sealing, snow plowing, snow removal, sanding, care of drainage, upkeep and repair of bridges, culverts, curbs, benches, bicycle and pedestrian facilities, street sweeping and cleaning, repair of damage and cleaning up after storms and traffic accidents, control of roadside vegetation, care of landscaped areas, planters, trees or other ornamental plantings, and upkeep and operation of traffic control devices, all in the manner as hereinafter specified.

**a. Roadway**

- (1) Surface Repair: The patching of holes, depressed areas, spot sealing, undersealing, etc.
- (2) Crack Sealing: The cleaning, filling and sealing of cracks in pavement with sealing compounds.
- (3) Sweeping and Cleaning: The removal of dirt or litter normally coming onto the roadway from action of traffic or from natural causes, such as flood and storm debris.
- (4) Snow Removal: The removal of snow from the roadway by plowing, sweeping, and hauling and shall include applying sand and/or salt when required. The hauling away of snow need only apply on those highway sections where snow storage is limited or at such times when accumulations become greater than storage area capacity.
- (5) Utilities: Including manholes, boxes or other appurtenances shall be maintained by their owners.
- (6) Storm Sewers: Shall be kept clean and free from debris; traps and sumps cleaned as required after each storm.
- (7) Culverts: Shall be kept clean and free from debris; inlets and outlets shall be kept free of debris and growing grass or brush.

**b. Bridges**

- (1) Structures shall be inspected in accordance with the national inspection standards of *U.S. Code, Section 116(d), Title 23*, administered by the State. Bridges designed to AASHTO H-20 or better standards must be inspected on a frequency not to exceed two years. Bridges that are posted for restricted weight limits and/or designed to AASHTO HS-15 or less will be inspected on an annual basis. Inspections are to be accomplished by a qualified inspector. The State's District Engineer shall be immediately notified of major defects. See current edition of *AASHTO Manual for Maintenance Inspection of Bridges* for inspector's qualifications, inspection reporting procedures, and structural analysis for load capacity of bridges.
- (2) For Structure Cosmetics, inside of the pedestrian underpass and on top of the pedestrian walks of the main structure shall be kept free of graffiti and debris.

**c. Improved Roadsides**

- (1) Curbs: Shall be kept in repair by cleaning, patching, lifting, and aligning.
- (2) Bicycle/Pedestrian Facilities: Shall be maintained in a usable condition for their intended purpose.
- (3) Lawn or Grass Areas: Shall be kept mowed, watered, edges trimmed, and the watering operations shall not flood or sprinkle on the roadway. No plants, trees, or bushes will be allowed in the area between the roadway and shared use path within the highway right-of-way unless otherwise approved through the State permitting process
- (4) Benches and Planters: Shall be kept in repair by cleaning, patching, aligning, and painting.

**d. Unimproved Roadsides**

- (1) Ditchings: Foreslopes, backslopes, and ditches shall be bladed and ditched regularly as required to keep as near as possible to the original typical cross section.
- (2) Cleaning: Foreslopes and backslopes shall be mowed as required. Trees and shrubs shall be kept trimmed, dead material removed and hazardous limbs pruned, waterways shall be kept free of debris.

### e. Traffic Control Devices

Traffic control devices installed and maintained on the urban extensions of the State Highway System shall be in conformance with the recommendations and specifications of the current *Manual on Uniform Traffic Control Devices for Streets and Highways* as approved by the American Association of State Highway and Transportation Officials (AASHTO) and as adopted by the Idaho Transportation Department. The maintenance to be performed on these items shall consist of furnishing all necessary labor, material, services, and equipment to install, replace, operate, and/or repair in accordance with this agreement or as otherwise permitted

*All traffic control devices installed inside the full control of access limits of the Highway System shall be the responsibility of the State.*

- (1) Route Guide Signing: This includes all official designation guide signs at junctions of the urban extensions of the State Highway System, all entering community signs and all U.S. or State Highway System route markers necessary to properly identify and keep the motorist sure of the routes.
- (2) Other Guide Signs: This includes all other guide signs of an informational nature identifying streets, city parks, landmarks, and items of geographical or cultural interest that the community desires to sign.
- (3) Warning Signs: These will include all signs used to indicate conditions that are actually or potentially hazardous to users of the highway or street.
- (4) Speed Signs: These will include all regulatory signs to indicate speed limits that have been designated in accordance with statutory provisions.
- (5) Other Regulatory Signs: These will include all regulatory signs, other than the speed sign and lane control sign which are used to indicate the required method of traffic movement or use of the public highway or street.
- (6) Highway Lighting: This includes all fixed illumination of the roadway or sidewalks for purposes of providing better visibility of persons, vehicles or roadway features. All highway lighting shall be installed and maintained in accordance with current policies of the State. Maintenance shall include all upkeep of supports, interconnecting service, electrical energy costs, cleaning, lamp renewal, and associated labor and material costs required to maintain the lighting system in continuous nighttime operation.
- (7) Lane-Line Markings: These will include those lines dividing the roadway between traffic moving in opposite directions, lane-lines separating two or more lanes of traffic moving in the same direction, painted channelization, pavement edge markings, and no passing barrier lines where required.
- (8) Other Pavement Markings: These include all stop lines, crosswalk lines, parking space limits and word and symbol marking set into or applied upon the pavement surface or curbing or objects within or adjacent to the roadway for the purpose of regulating or warning traffic.

## 11. ENCROACHMENT PERMITS

If the State delegates authority to issue encroachment permits to the City, the authority shall pertain to all parts of the highway or street throughout the particular length indicated under Section 14 of this agreement. Authority to issue encroachment permits shall not be assigned to the City unless they have adequate ordinances governing the encroachments together with an administrative organization and procedure capable of enforcing the ordinances.

Permits shall be issued on a form provided by the State and the City will furnish a copy of each permit to the State. The City agrees to follow current policies of the State regarding encroachment unless the City, by ordinance or other regulation, imposes more restrictive regulations as stated below. Prior approval of the State shall be secured before any permit is issued for the original installation of any utility line, driveway or other permanent encroachment within the highway right-of-way.

If the City, by ordinance or other regulation, imposes more restrictive regulations and requirements regarding signs, marquees and/or driveways than above set forth or as provided in current State policies, nothing in these provisions shall be construed to prevent the City from enforcing such restrictive regulations in the granting or refusing of permits with respect to any State Highway. Where authority to issue encroachment permits is retained by the State, all local ordinances which are more restrictive than State policy will be observed. When authority to issue Encroachment permits is retained by the State, approval of the City will be secured prior to the issuance of a permit. State permit forms will be used and a copy will be forwarded to the City for its record.

The City or State shall comply with its usual policy with respect to collecting costs from permittees in such cases as fees or charges are made by the City or State for encroachment work on streets or highways.

The City is exempt from encroachment permit fees from the State. As well, the State is exempt from encroachment permit fees from the City.

No signs, billboards or structures other than those authorized and installed by the State or the City as necessary for the regulating, warning, and guiding of traffic shall be permitted within or to overhang the right-of-way of any State Highway, except in accordance with these provisions:

- a. Signs or marquees extending over the sidewalk and right-of-way may be installed on a permitted basis in business districts only, subject to the following restrictions:
  - No sign or marquee shall be permitted to project over the roadway nor to extend beyond a vertical line located 18 inches from the inside face of the curb away from the roadway.
  - Signs extending over the sidewalk area shall have no part thereof less than 12 feet above sidewalk or ground level. Marquees extending over the sidewalk area shall have no part thereof less than eight feet above sidewalk or ground level.
- b. Displays or signs overhanging the right-of-way may be authorized on a permit basis only outside of business districts when the display is placed flat against and supported by the building and providing it does not extend more than 12 inches into the right-of-way.
  - (1) Displays or signs already permitted into the State right-of-way and not impacted by the current project will be allowed to remain under their current permit.
- c. All signs and marquees shall conform to the city building and/or sign code excepting that minimum clearance requirements as herein specified must be complied with.
 

They shall at all times be maintained in a good appearing and structurally safe condition. Any existing sign or marquee suspended or projected over any portion of State Highway right-of-way, which constitutes a hazard, shall be immediately repaired or removed.
- d. Signs or displays will not be permitted which resemble, hide, or because of their color, interfere with the effectiveness of traffic signals and other traffic control devices. Illuminated signs or displays containing red, yellow, or green lights will not be permitted to overhang the right-of-way.
- e. Temporary municipal decorations may be installed and suspended over the State Highway travel lanes on a permit basis only. They shall not be permitted in locations that interfere with the visibility and effectiveness of traffic control devices.

It is understood that none of the provisions listed above (a. to e. inclusive) will be in conflict the Beautification of Highways Act of 1966, Idaho Code, Section 40, Chapter 28.

- f. Use of state highway right-of-way for benches, planters, and trees is subject to the following conditions:
  - Benches, planters, and trees must be at least 18 inches from the face of the curb. When benches, planters, and trees are placed on sidewalks, there must be a four-foot open space for pedestrians and bicyclists measured at a right angle from the edge of the sidewalk, or as an alternative, spacing that meets city-approved standards. All other items shall be allowed through the State permitting process.
  - Benches, planters, and trees should not obstruct crosswalks or wheelchair ramps, or force pedestrians into the street by their placement.
  - Benches, planters, and trees should not be placed so as to impede the sight distance of vehicles using the highway.
  - Benches, planters, and trees shall not bear markings or signs that resemble official traffic signs.
  - Cities allowing benches, planters, and trees on state highway right-of-way agree to indemnify, defend regardless of outcome, and hold harmless, ITD from all accidents or occurrences resulting in damage to property, injury, or loss of life related to bench placement on highway right-of-way within the city.

**12. TRANSPORTATION PERMITS**

Transportation permits will be required on State Highways for all vehicles and their loads that exceed legal limitations. If authority to issue transportation permits is delegated to the City, such authority shall pertain only to travel that originates and terminates within the City corporate limits.

**13. ROUTE DESCRIPTION**

<u>Route No.</u>	<u>Milepost</u>	<u>Length Miles</u>	<u>Description of Routing</u>
SH-41	0.446 - 3.450	3.004	Main Route between Post Falls and Rathdrum
*City Streets Within State Right-of-way limits	Various	Various	Wyoming Ave., Lancaster Rd., Nagel Rd., California St., Boekel Rd., OK Corral (Future), Shared use <b>path</b>

\*This includes portions of City streets that intersect SH-41 or drainage facilities associated with them that will be constructed as part of this project.

**14. DELEGATION OF MAINTENANCE**

The maintenance work to be performed by the City or State shall conform to the provisions hereof and shall include those operations as hereinafter indicated.

MAINTENANCE FUNCTION	AGENCY TO PERFORM WORK	
	SH-41	*City Streets Within State Right-of-way limits
ROADWAY		
1. Surface Repair	<u>State</u>	<u>City</u>
2. Crack Sealing	<u>State</u>	<u>City</u>
3. Sweeping and Cleaning	<u>State</u>	<u>City</u>
4. Snow Removal	<u>State</u>	<u>City</u>
5. Utilities	<u>City/Utility Companies</u>	<u>City/Utility Companies</u>
6. Culverts	<u>State</u>	<u>City</u>
7. Storm Sewers	<u>State/City</u>	<u>City</u>

**MAINTENANCE FUNCTION**

**AGENCY TO PERFORM WORK**

	SH-41	*City Streets Within State Right-of-way limits
<b>BRIDGES</b>		
1. Main Structure	<u>State</u>	<u>N/A</u>
2. Structure of the Pedestrian Walks on the Main Structure	<u>State</u>	<u>City</u>
3. Structure of the Pedestrian Underpass at the Main Structure	<u>State</u>	<u>City</u>
4. Cosmetics of the Pedestrian Underpass at the Main Structure	<u>City</u>	<u>City</u>
5. Cosmetics of the Pedestrian Walks at the Main Structure	<u>City</u>	<u>City</u>
<b>IMPROVED ROADSIDES</b>		
1. Curbs	<u>State</u>	<u>City</u>
2. Sidewalk	<u>City</u>	<u>City</u>
3. Lawn or Grass Areas	<u>City</u>	<u>City</u>
4. Trees and Planting	<u>City (By Permit)</u>	<u>City</u>
5. Medians	<u>City</u>	<u>City</u>
6. Benches and Planters	<u>City</u>	<u>City</u>
<b>UNIMPROVED ROADSIDES</b>		
1. Ditching	<u>State</u>	<u>City</u>
2. Cleaning	<u>State</u>	<u>City</u>
3. Weed Eradication	<u>State</u>	<u>City</u>
<b>TRAFFIC CONTROL DEVICES</b>		
1. Route Guide Signs	<u>State</u>	<u>City</u>
2. Other Guide Signs	<u>State</u>	<u>City</u>
3. Warning Signs	<u>State</u>	<u>City</u>
4. Rectangular Rapid Flashing Beacon (RRFB)	<u>N/A</u>	<u>N/A</u>
5. Speed Signs	<u>State</u>	<u>City</u>
6. Other Regulatory Signs	<u>State</u>	<u>City</u>
7. Highway Lighting/Illumination	<u>City</u>	<u>City</u>
8. Traffic Signals		
9. Lane-Line Markings	<u>State</u>	<u>City</u>
10. Other Pavement Markings		
Parking Space Limits	<u>N/A</u>	<u>City</u>
Crosswalks	<u>State</u>	<u>City</u>
Stop Bars	<u>State</u>	<u>City</u>
School Crossing	<u>State</u>	<u>City</u>
Lane Control	<u>State</u>	<u>City</u>
ISSUE PERMITS ENCROACHMENTS	<u>State</u>	<u>City</u>
ISSUE PERMITS TRANSPORTATION	<u>State</u>	<u>City</u>

\* The State’s application of wildland grass seed will be considered “Unimproved” Roadside. If the City causes roadside areas to be improved to lawn or grass areas, maintenance of these areas will be the responsibility of the City.

\*\* Storm Sewers that are on private property and City right-of-way will be maintained by the City. Any additional peak load to the storm water system in the Project Section due to development will only be allowed with ITD and City approval.

\*\*\*Future bus and transit maintenance will be covered in future agreements or in an addendum to this agreement.

**15. SUBSEQUENT IMPROVEMENTS**

When a highway section or portion thereof is improved to urban standards, i.e., with curbs, sidewalks, etc., the delegation of maintenance shall automatically change to conform to the provisions as provided for similar sections under this agreement.

**16. FINANCING**

ITD and the City shall bear all costs of maintenance obligations assigned to them under this Memorandum of Understanding.

**17. LIMITATIONS**

Nothing in this Memorandum of Understanding between ITD and the City shall be construed as limiting or expanding the statutory or regulatory responsibilities of any involved individual in performing functions granted to them by law; or as requiring either entity to expend any sum in excess of its respective appropriation. Each and every provision of this Memorandum is subject to the laws and regulations of the state of Idaho and of the United States.

Nothing in this Memorandum of Understanding shall be construed as expanding the liability of either party. In the event of a liability claim, each party shall defend their own interests. Neither party shall be required to provide indemnification of the other party.

**18. EFFECTIVE DATE:**

This Memorandum of Understanding shall become effective upon signature of the Director of ITD or delegate and the signing authority of the City of Rathdrum, whichever is most recent, and shall remain in full force and effect until amended or terminated.

**19. METHOD OF TERMINATION:**

This Memorandum of Understanding shall remain in force unless formally terminated by mutual agreement of each party after thirty (30) days written notice to other party.

**20. AMENDMENTS:**

Amendments to this Memorandum shall become effective upon mutual agreement and written approval by the Director of ITD or delegate and the signing authority of the City of Rathdrum.

**21. SIGNATURES:**

**IDAHO TRANSPORTATION DEPARTMENT**

By \_\_\_\_\_  
Director or Delegate

Date \_\_\_\_\_

**THE CITY OF RATHDRUM**

By \_\_\_\_\_

Date \_\_\_\_\_

(Title) \_\_\_\_\_



## BICYCLE/PEDESTRIAN FACILITIES

The Idaho Transportation Department is committed to achieving a safe, effective, and balanced multimodal transportation system that includes accommodations for bicyclists, pedestrians, and pedestrians with disabilities, along with motorized transportation modes. This commitment includes developing the transportation infrastructure to improve conditions for bicycling and walking by integrating provisions for bicycles and pedestrians into new construction and reconstruction highway projects through design features appropriate for the context and function of the transportation facility. The design and construction of facilities shall anticipate likely future demand for bicycling and pedestrian facilities, and not preclude the provision of future improvements.

### Definitions

A **“bike/bicycle lane”** is a portion of a roadway that has been designated with signing and pavement markings for the preferential or exclusive use of bicyclists.

A **“shared use path”** is a multiuse facility for use by pedestrians and/or bicyclists that is physically separated from motorized vehicular traffic by an open space or barrier, and is within either the highway right-of-way or an independent right-of-way.

A **“sidewalk”** is that portion of a roadway that is intended for pedestrian use, and lies between the curb lines or the lateral lines of the travel way and the adjacent property lines.

An **“Accommodation”** is any facility, design feature, operational change, or maintenance activity that improves the environment in which bicyclists and pedestrians travel.

### Facilities

Due consideration shall be given to bicycle and pedestrian needs in the design of new transportation facilities. The following items shall be considered when determining the possible inclusion of bicycle or pedestrian facilities within a project.

- The project’s scope
- Relevant planning documents, such as a corridor plan, local transportation plan, local pedestrian/bicycle policy, or facilities plan
- Limitations due to historic structures, environmental constraints, or other unique project features
- Context-sensitive issues, such as school crossings, transit stops, etc.
- Americans with Disabilities Act (ADA) requirements
- Discussions with local governments regarding any special circumstances, such as high-use recreation traffic generators outside of a city limit (schools, churches, business parks, etc.).

The above list does not represent all possible guidance to be considered when making a determination.

When it has been determined that a bicycle or pedestrian facility should be included within the project, accommodations generally include:

- Inside city limits - consideration of sidewalks and widened outside travel lanes or bicycle lanes.
- Outside city limits - the use of roadway shoulders.

All consideration given to bicycle and pedestrian facilities shall be documented in the project's Concept Report.

### **Project Costs**

When the Concept Report requires the construction of bicycle and/or pedestrian facilities for projects on the State highway system, all costs associated with the construction shall be distributed in accordance with Administrative Policy A-19-01, FINANCING CONSTRUCTION OF STATE HIGHWAYS IN CITIES. Off-system and local bicycle and/or pedestrian facilities shall be the responsibility of the local entity, unless otherwise specified in a state/local agreement executed prior to construction.

### **Maintenance**

The Department is responsible for costs associated with the maintenance of bicycle lanes on the State highway system, unless otherwise specified in a state/local maintenance agreement.

Routine maintenance of sidewalks and separated pathways located on highway right-of-way shall be the responsibility of the appropriate local agency through an agreement completed prior to construction. At its discretion, the local agency may accomplish certain maintenance activities through organized groups or entities that it authorizes. However, the maintenance responsibility remains with the local government agency.

### **Projects Proposed by Others**

Due to the localized nature of non-motorized trips, the Department encourages local units of government to participate in planning and developing infrastructure that will support walking and bicycling.

The Department supports local governments by considering requests to make highway right-of-way available for non-motorized facilities. Future highway expansion or interference with the operational characteristics of the highway may preclude ITD from approving such requests.

Prior to giving approval for a facility, the Department may require the requesting agency to provide detailed analysis of the proposed facility's impacts to the highway in order to determine the acceptability of the facility.

When appropriate, the Department shall negotiate the use of state highway right-of-way only with local governments or other public agencies, not with private groups or organizations. This is to insure that project development, funding, and maintenance issues can be coordinated by an agency that can make a long-term written agreement with ITD. Private groups or organizations may participate as part of the planning process, but only local governments shall be responsible for the facility's planning, construction, and maintenance.

Signed \_\_\_\_\_

Date: August 05, 2009

L. Scott Stokes, P.E.  
Acting Director

This Policy is based on:

- Title 23, USC Section 135
- SAFETEA-LU Section 6001
- Section 504 of the Rehabilitation Act of 1983, as amended, 29 USC 792
- Title II, Americans with Disability Act
- Board Policy B-09-08, BICYCLE/PEDESTRIAN FACILITIES
- Board Policy B-13-03, ENVIRONMENTAL STEWARDSHIP

Department-wide supervision and coordination assigned to:

- Division of Public Transportation Administrator

Direction for activity and results assigned to:

- Bicycle/Pedestrian Coordinator

Direction for standards assigned to:

- Geometrics Engineer

Department procedures contained in:

- This policy
- Statewide Bicycle Plan
- ITD Design Manual

Former date of A-09-08:

7/1/93 (Formerly numbered A-09-08, which replaced A-09-04, TRAVELWAYS FOR NON-MOTORIZED TRANSPORTATION)

Cross-reference to related Administrative policies:

- A-05-16, MAINTENANCE OF STATE HIGHWAYS
- A-11-01, TRANSPORTATION IMPROVEMENT PROGRAM
- A-19-01, FINANCING CONSTRUCTION OF STATE HIGHWAYS IN CITIES

**PUBLIC WORKS DEPARTMENT**

8047 W. Main Street  
Rathdrum, ID 83858  
P 208.687.2700  
F 208.687.1377

# Memo

**To:** Rathdrum City Council  
**From:** Kevin Jump  
**CC:**  
**Date:** March 5, 2020  
**Re:** Intersection Improvements at Lancaster Road & Meyer Road

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There is a commercial development project proposed on the southeast corner of Lancaster Road and Meyer Road. Through various annexations and platting processes, the City of Rathdrum has obtained/reserved public right-of-way from all 4 quadrants of the intersection. The obtained right-of-way was based upon a superimposed dual-lane roundabout footprint.

With the public right-of-way having already been obtained, recent accident history and a commercial development project being proposed at the southeast corner, we believe that it is now appropriate to make major intersection improvements. Further, we are considering that the commercial development contractor completes the Lancaster-Meyer intersection improvements with reimbursement from the City of Rathdrum's Traffic Circulation Impact Fee Fund. We believe that there is mutual benefit in having these intersection improvements completed simultaneously with the commercial development project. Further, we believe that there is significant benefit with utilizing the same engineering consultant to design both the intersection improvements and the nearby/adjoining site development project (SE Corner).

For your consideration, I'm herein attaching a Scope of Work and Work Order for engineering design services. I'm herein seeking approval by the Rathdrum City Council to advance this work and grant authorization to Mayor Holmes for executing the Work Order.

## SCOPE OF SERVICES

### **Roundabout Design - Lancaster & Meyer Estimated Costs**



#### **1. Topographic Survey**

- Research existing utilities (size, location, depth, etc...) based on Record Drawings
- Call U-Dig hotline for locates prior to field work
- Locate and tie existing features, structures on property
- Download points and prepare CAD Basemap

#### **2. Right-of-Way Acquisition**

- Assess R/W acquisition needs
- Perform Boundary / Right-of-Way Survey as required
- Prepare Land Descriptions for R/W acquisitions from impacted parcels
- Stake new footprint of R/W to indicate the to-be-acquired right-of-way to surrounding property owners
- Coordinate with surrounding property owners as required
- Assist City with negotiations as needed

#### **3. Utility Relocation**

- Assess existing utilities, locations, sizes, etc.
- Determine utility conflicts, or utilities that need to be relocated
- Coordinate with impacted utility company
- Review utility relocation agreements, etc.

#### **4. Construction Drawings & Specifications**

- Meetings with City staff and jurisdictional agencies to discuss project concepts. Refine project layouts based on comments and work with agencies towards approvals / entitlements
- Create CAD Basemap drawing and import preliminary linework/design from City
- Verify vehicle turning movements and prepare dimensional analysis.
- Prepare Construction Drawings & Specifications including Grading Plan, Stormwater Calculations, Utility Plan, Lighting Plan, Signage & Striping Plan, and Project Details & Specifications
- Review plans with City and explore value engineering options
- Submit Plans to City and affected agencies for review and/or approval
- Coordinate with agencies on plan comments as required
- Make revisions to plans as required by jurisdictional agencies
- Obtain Construction Drawing approval
- QA / QC and coordinate with City

**5. Bid Assistance**

- Provide City assistance in bidding of project as necessary

**TOTAL ESTIMATED PROJECT COST: \$57,750.00**

**EXCLUSIONS**

- Construction Staking (except as noted in Section 2)
- Construction Management
- Title, application & agency fees
- Reproduction costs

# WORK ORDER

February 18, 2020

**PROJECT NAME:** Roundabout Design - Lancaster & Meyer

**LCE JOB #:** 19-068

**LOCATION:** Rathdrum, Idaho

**PARCEL # / AIN:** N/A



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## BILLING / CONTACT INFORMATION:

**CONTACT PERSON:** Kevin Jump, PE

**COMPANY:** City of Rathdrum

**ADDRESS:** 8047 West Main Street  
Rathdrum, ID 83858

**BUS PHONE:** (208) 687-0261

**CELL PHONE:**

**EMAIL:** kevin@rathdrum.org

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## PROJECT DESCRIPTION / SCOPE:

See attached Scope of Services. Scope does not include construction staking or construction management services, application/agency fees or reproduction costs. Engineer will be available for bidding assistance if needed.

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**ESTIMATED COST:** \$ 57,750.00     T&M     LUMP SUM

**REQUIRED DEPOSIT:** \$ 0.00

**PREPARED BY:** \_\_\_\_\_



**DATE:** 2/17/2020

**ACCEPTED BY:** \_\_\_\_\_

**DATE:** \_\_\_\_\_

**PUBLIC WORKS DEPARTMENT**

8047 W. Main Street  
Rathdrum, ID 83858  
P 208.687.2700  
F 208.687.1377

# Memo

**To:** Rathdrum City Council  
**From:** Kevin Jump  
**CC:**  
**Date:** March 5, 2020  
**Re:** Acceptance of Public Right-of-Way for Deadwood Street

---

The attached legal description and conveyance documents serve as a right-of-way replacement for the recently-vacated right-of-way in/adjoining the Corbin Crossing South development.

For your consideration, I'm herein seeking approval by the Rathdrum City Council to accept this public right-of-way and authorize Mayor Holmes to execute the conveyance documents.

**RECORDING REQUESTED BY:**

Copper Basin Construction, Inc.  
PO Box 949  
Hayden, ID 83835

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**GRANT OF RIGHT-OF-WAY**

KNOWN ALL MEN BY THESE PRESENTS, that **COPPER BASIN CONSTRUCTION, INC.**, PO Box 949, Hayden, ID 83835 and **BIG CREEK LAND COMPANY, LLC**, 1950 W. Bellerive Lane #107, Coeur d'Alene, ID 83814, the Grantors, for and in consideration of the sum of One Dollar (\$1.00) and other good and valuable consideration, in hand paid by the City of Rathdrum, Kootenai County, State of Idaho, the receipt of which is hereby acknowledged, do hereby grant, quitclaim and convey unto the said City of Rathdrum, 8047 W. Main Street, Rathdrum, ID, 83858, a municipal corporation, (herein the "Grantee"), a public right-of-way for the construction, improvement, operation and maintenance of public roadway, allowing also placement and maintenance of pipelines for water and sewer, and such other surface and underground utility lines as may be necessary, upon and across the following described property, to wit:

*See Exhibits A & A-1 attached hereto.*

Subject to: Any existing easements, covenants, conditions, rights, reservations, restrictions or encumbrances of record or in view.

TO HAVE AND TO HOLD SUCH RIGHT-OF-WAY FOR PUBLIC PURPOSES, the Grantors do hereby dedicate all interest in said strip of land to public use for such purposes.

IN WITNESS OF THE ABOVE, the Grantors have caused this instrument to be executed this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

**GRANTOR – COPPER BASIN CONSTRUCTION, INC.**

\_\_\_\_\_

Steve White, President  
Copper Basin Construction, Inc.

**ACKNOWLEDGMENT – COPPER BASIN CONSTRUCTION, INC.**

STATE OF IDAHO )  
 )ss.  
COUNTY OF KOOTENAI )

On this \_\_\_\_ day of \_\_\_\_\_ 20\_\_\_\_, before me, the subscriber, a Notary Public in and for said State and County, personally appeared Steve White, the President of Copper Basin Construction, Inc., known or subscribed and sworn to me to be the person whose name is subscribed to the within instrument, and in due form of law acknowledged that he is authorized on behalf of said corporation to execute all documents pertaining hereto and acknowledged to me that he executed the same as his voluntary act and deed on behalf of said corporation.

**IN WITNESS WHEREOF**, I have hereunto set my hand and affixed my seal in said State and County on the day and year last above written.

\_\_\_\_\_  
Notary Public

Residing at: \_\_\_\_\_

My Commission Expires: \_\_\_\_\_

**GRANTOR – BIG CREEK LAND COMPANY, LLC**

\_\_\_\_\_  
Clifford E. Mort, Member  
Big Creek Land Company, LLC

**ACKNOWLEDGMENT – BIG CREEK LAND COMPANY, LLC**

STATE OF IDAHO )  
 )ss.  
COUNTY OF KOOTENAI )

On this \_\_\_\_ day of \_\_\_\_\_ 20\_\_\_\_, before me, the subscriber, a Notary Public in and for said State and County, personally appeared Clifford E. Mort, a Member of Big Creek Land Company, LLC, known or subscribed and sworn to me to be the person whose name is subscribed to the within instrument, and in due form of law acknowledged that he is authorized on behalf of said limited liability company to execute all documents pertaining hereto and acknowledged to me that he executed the same as his voluntary act and deed on behalf of said limited liability company.

**IN WITNESS WHEREOF**, I have hereunto set my hand and affixed my seal in said State and County on the day and year last above written.

\_\_\_\_\_  
Notary Public

Residing at: \_\_\_\_\_

My Commission Expires: \_\_\_\_\_

**ACCEPTANCE**

The undersigned, in his capacity as Mayor for the City of Rathdrum, hereby accepts the Grant of Right-of-Way described herein.

**CITY OF RATHDRUM**

By: \_\_\_\_\_  
Vic Holmes, Mayor

Dated: \_\_\_\_\_

ATTEST:

\_\_\_\_\_  
Sherri I. Halligan, City Clerk

STATE OF IDAHO )  
 )ss.  
COUNTY OF KOOTENAI )

On this \_\_\_\_ day of \_\_\_\_\_ 20\_\_\_\_, before me, the subscriber, a Notary Public in and for said State and County, personally appeared Vic Holmes, as Mayor of the City of Rathdrum, known or subscribed and sworn to me to be the person whose name is subscribed to the within instrument, and who acknowledged that he had authority to execute the same.

**IN WITNESS WHEREOF**, I have hereunto set my hand and affixed my seal in said State and County on the day and year last above written.

\_\_\_\_\_  
Notary Public

Residing at: \_\_\_\_\_

My Commission Expires: \_\_\_\_\_

STATE OF IDAHO

)

)ss.

COUNTY OF KOOTENAI

)

On this \_\_\_\_ day of \_\_\_\_\_ 20\_\_\_\_, before me, the subscriber, a Notary Public in and for said State and County, personally appeared Sherri I. Halligan, as City Clerk of the City of Rathdrum, known or subscribed and sworn to me to be the person whose name is subscribed to the within instrument, and who acknowledged that she had authority to execute the same.

**IN WITNESS WHEREOF**, I have hereunto set my hand and affixed my seal in said State and County on the day and year last above written.

\_\_\_\_\_  
Notary Public

Residing at: \_\_\_\_\_

My Commission Expires: \_\_\_\_\_

## EXHIBIT A

A parcel of land being a portion of the Southwest Quarter of Section 5, Township 51 North, Range, 4 West, Boise Meridian, City of Rathdrum, Kootenai County, Idaho, and more particularly described as follows:

**Commencing at** the South Quarter corner of said Section 5, being a 2.5 inch aluminum cap marked "PLS 11187" per CP&F Instrument Number 2406313000, Records of Kootenai County, Idaho, from which the Center Quarter corner bears North  $00^{\circ}39'02''$  East 2659.73 feet; thence North  $59^{\circ}47'26''$  West 1093.85 feet a point on the easterly Right-of-Way of Railway Avenue, being the **True Point of Beginning**;

thence along said easterly Right-of-Way, along a non-tangent curve to the left with a radius of 2910.20 feet, an arc length of 60.00 feet, a central angle of  $1^{\circ}10'53''$  and a long chord that bears North  $01^{\circ}51'16''$  West 60.00 feet;

thence leaving said easterly Right-of-Way, North  $88^{\circ}08'44''$  East 3.31 feet;

thence along a non-tangent curve to the left with a radius of 300.00 feet, an arc length of 87.36 feet, a central angle of  $16^{\circ}41'01''$  and a long chord that bears North  $79^{\circ}48'13''$  East 87.05 feet;

thence North  $71^{\circ}27'43''$  East 56.73 feet;

thence along a non-tangent curve to the right with a radius 500.00 feet, an arc length of 85.54 feet, a central angle of  $9^{\circ}48'08''$  and a long chord that bears North  $76^{\circ}21'47''$  East 85.44 feet;

thence North  $81^{\circ}15'51''$  East 260.08 feet;

thence along a non-tangent curve to the right with a radius of 1000.00 feet, an arc length of 175.37 feet, a central angle of  $10^{\circ}02'53''$  and a long chord that bears North  $86^{\circ}17'18''$  East 175.15 feet;

thence South  $88^{\circ}41'16''$  East 81.26 feet;

thence along a non-tangent curve to the left with a radius of 20.00 feet, an arc length of 31.42 feet, a central angle of  $90^{\circ}00'00''$  and a long chord that bears North  $46^{\circ}18'44''$  East 28.28 feet;

thence South  $87^{\circ}43'23''$  East 60.01 feet;

thence along a non-tangent curve to the left with a radius of 20.00 feet, an arc length of 15.50 feet, a central angle of  $44^{\circ}24'55''$  and a long chord that bears South  $20^{\circ}53'43''$  East 15.12 feet;

thence along a non-tangent reverse curve to the right with a radius 50.00 feet, an arc length of 156.06 feet, a central angle of  $178^{\circ}49'50''$  and a long chord that bears South  $46^{\circ}18'44''$  West 99.99 feet;

thence along a non-tangent curve to the left with a radius of 20.00 feet, an arc length of 15.50 feet, a central angle of 44°24'55" and a long chord that bears North 66°28'48" West 15.12 feet;

thence North 88°41'16" West 82.27 feet;

thence along a non-tangent curve to the left with a radius of 940.00 feet, an arc length of 164.85 feet, a central angle of 10°02'53" and a long chord that bears South 86°17'18" West 164.64 feet;

thence South 81°15'51" West 260.08 feet;

thence along a non-tangent curve to the left with a radius of 440.00 feet, an arc length of 75.28 feet, a central angle of 9°48'08" and a long chord that bears South 76°21'47" West 75.18 feet;

thence South 71°27'43" West 56.73 feet;

thence along a non-tangent curve to the right with a radius of 360.00 feet, an arc length of 104.83 feet, a central angle of 16°41'01" and a long chord that bears South 79°48'13" West 104.46 feet;

thence South 88°08'44" West 3.31 feet to the **True Point of Beginning**;

containing 1.220 acres of land, more or less;

as depicted on **EXHIBIT A-1**.



CENTER QUARTER CORNER  
 FOUND 2" ALUMINUM CAP PER CP&F  
 INSTRUMENT #1259228



TAX # 20934,  
 EX TAX'S IN  
 THE  
 SOUTHWEST  
 QUARTER

VACATED 60' UNNAMED RW  
 PER ORDINANCE NUMBER 586  
 INSTRUMENT NUMBER 2731045000

N0° 39' 02"E 2659.73'

LOT 6  
 BLOCK 14

CORBIN CROSSING SOUTH  
 3RD ADDITION  
 (BOOK L / PAGE 355)

TRUE  
 POINT OF  
 BEGINNING

RAILWAY AVENUE

TAX# 20937,  
 EX PLATTED  
 PORTION,  
 EX RIGHT-OF-WAY  
 IN THE  
 SOUTHWEST  
 QUARTER

N59° 47' 26"W 1093.85'



**LEGEND**

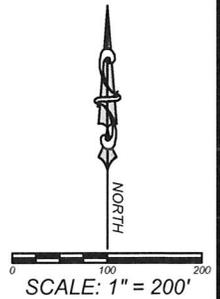
- △ CALCULATED POINT,
- △ NOTHING FOUND OR SET

Curve Table					
Curve #	Length	Radius	Delta	Bearing	Chord
C1	60.00'	2910.20'	001°10'53"	N01°51'16"W	60.00'
C2	87.36'	300.00'	016°41'01"	N79°48'13"E	87.05'
C3	85.54'	500.00'	009°48'08"	N76°21'47"E	85.44'
C4	175.37'	1000.00'	010°02'53"	N86°17'18"E	175.15'
C5	31.42'	20.00'	090°00'00"	N46°18'44"E	28.28'
C6	15.50'	20.00'	044°24'55"	S20°53'43"E	15.12'
C7	156.06'	50.00'	178°49'50"	S46°18'44"W	99.99'
C8	15.50'	20.00'	044°24'55"	N66°28'48"W	15.12'
C9	164.85'	940.00'	010°02'53"	S86°17'18"W	164.64'
C10	75.28'	440.00'	009°48'08"	S76°21'47"W	75.18'
C11	104.83'	360.00'	016°41'01"	S79°48'13"W	104.46'

Line Table		
Line #	Bearing	Length
L1	N88°08'44"E	3.31'
L2	N71°27'43"E	56.73'
L3	S88°41'16"E	81.26'
L4	S87°43'23"E	60.01'
L5	N88°41'16"W	82.27'
L6	S71°27'43"W	56.73'
L7	S88°08'44"W	3.31'

LANCASTER ROAD

SOUTH QUARTER CORNER  
 FOUND 2-1/2" ALUMINUM CAP  
 MARKED "PLS 11187" IN MONUMENT  
 WELL PER CP&F 2406313000



126 E. POPLAR AVENUE  
 COEUR D'ALENE, IDAHO 83814  
 PHONE: 208.676.0230  
 WWW.LAKECITYENGINEERING.COM

**EXHIBIT A-1**  
 PORTION OF THE SW/4

S5, T51N, R4W, BM, CITY OF RATHDRUM  
 KOOTENAI COUNTY, IDAHO

DESIGNED BY:	DCD
DRAFTED BY:	WAL
SCALE:	1" = 200'
DATE:	01/30/2020
JOB NO:	LCE 20-016
FILE:	20-016 ROW XHBT.dwg





County Recorder on February 1, 2011 (attached hereto in Exhibit G).

**Comprehensive Plan Designation:** The Future Land Use Map identifies the site as Transformational. Residential and Commercial uses are compatible with this designation.

## **II. PROJECT REVIEW**

City planning and public works staff and the Planning and Zoning Commission have reviewed the proposed subdivision. This report reflects an overview of the proposed preliminary subdivision based on the submitted application materials, public hearing for the proposal, comments received, the record from the Annexation request and hearings from 2011, the Planning and Zoning Commission's recommendation, and other information available at the time of this report. The project is subject to modification prior to final action of the Council.

### **Comprehensive Plan Consistency Analysis:**

The proposed development is generally consistent with and implements several goals and policies of the City's Comprehensive Plan. The relevant goals and policies established within the Rathdrum Comprehensive Plan that pertain to the proposed development are listed in Exhibit F, attached hereto. Included for each Comprehensive Plan element is an analysis of the proposed project's consistency.

### **Rathdrum City Code:**

Several titles of the Rathdrum City Code (RCC) provide regulations for the development and operation of the proposed project. Those portions of the RCC that pertain to the proposed project are described in Exhibit E, attached hereto. Included for each requirement is an analysis of the proposed project's conformance with that requirement.

### **Public Hearing / Public Comments:**

The Planning and Zoning Commission held a public hearing for this proposal on January 15, 2020 (see process information within Exhibit E, §12-3-1-6 attached hereto). Minutes from the hearing are attached within Exhibit H.

During the hearing City staff presented a brief report regarding the proposal, followed by the applicant's testimony and public testimony. The following public testimony was received:

1. Jim Dugan, 6302 W Mallory Road, Rathdrum, ID 83858  
Spoke Neutrally - Mr. Dugan lives just north of the project and is concerned about his livestock and questioned whether or not the developer will install a fence between the project and rural property to the north. He also commented that he uses Meyer Road for his main access and is concerned about the extra traffic this subdivision will generate.
2. John Leitzke, 5943 W Highway 53, Rathdrum, ID 83858  
Spoke Neutrally - Mr. Leitzke would like to see fencing installed and questioned just where the sewer lift station was going to be installed since he lives directly east of this subdivision.
3. Sally Williams, 5922 W Mallory Road, Rathdrum, ID 83858  
Spoke Neutrally - Mrs. Williams stated that she also lives just north of this project and would like to see a fence installed.
4. Richard Williams, 5922 W Mallory Road, Rathdrum, ID 83858  
Spoke Neutrally - Mr. Williams lives just north of this proposed subdivision and is concerned about the survey being done for this project and how will affect his property and is worried about the safety of his horses.

The applicant's representative, Ray Kimball of Whipple Consulting Engineers, Inc., provided rebuttal stating

that fencing has not been discussed by the developer, but they feel that most homeowners would put up their own fencing. Also, the developer has an agreement with the Lion's Club to provide their small RV park along Meyer Road with sewer stubs for each RV.

Staff clarified that any agreement with the Lion's Club by Viking is a private agreement to which the City is not a party and that RVs shall not be connected to the City's sewer system without City approval.

During deliberations the Commission requested more information from the applicant in relation to traffic at the intersection of State Highway 53 and Meyer Road and fencing and therefor continued deliberations to the next regular Planning and Zoning Commission meeting to be held on February 19, 2020.

Following continuation of the hearing, staff performed a review of the archived audio recording of the City Council hearing for the annexation of the property held on October 12, 2010. The following pertinent information in relation to the Commission's request for more information was found within the recording:

1. At approximately minute 22:00 Mr. Scott Krajack, representative for Viking, begins his public testimony and states on the record that all of the concerns which came up during the previous hearings for the proposal had been addressed – including the Lion's Club property being included in the annexation, the interim and permanent sewer infrastructure being installed up front so that the interim infrastructure could be removed once not needed, and the relationship with the neighbors to the north. He stated that he personally met with all of the adjoining property owners to the north and had walked the property with several of them and “at this point we have agreed to do some sort of sight obscuring fencing along that property line. My intention was to bring back the actual nature of that fence at the development which of course we have to come back to you for. So it has an agreement on, you know, how high, what type, things like that at that time. We also have agreed or, or I've I've agreed, to provide a buffer on our north property line, um, so that, so that this fencing would actually stay away from the property line.”... “At this point our intent is a minimum of a three foot buffer. It might be more. Once we actually get to development we'll do a boundary line survey so at that point we'll come back with the relationship between existing fences and property line. It might even be more than three feet if we had encroachments and things like that. The plan was to bring that all back at development.”
2. At approximately minute 27:00 City Planner, Chris Rife, read into the record all of the recommended conditions of approval which were in the staff report. These included the 6 staff recommended conditions (a through f below) and the two P&Z recommended conditions (g and h below) as follows:
  - a. Meyer Road Right of Way: Forty (40) foot of right-of-way east from the centerline of Meyer Road adjacent to the subject property be dedicated to the City and included in the annexation. Also annex twenty five foot of the existing County right-of way west from the centerline of Meyer Road.
  - b. Highway 53 Access: Vehicular access from Highway 53 shall be limited to no more than two points of access to service future residential and commercial development in the subject property Such access shall meet the standards, goals and policies of the Transportation Plan and as approved by the Idaho Transportation Department and the City. Corridor management is essential no matter what land use might be approved.
  - c. Sewer Service: Any proposed interim sewer plan shall be reviewed and approved by the City engineer and subsequently adopted by City Council as an amendment to the City's Sewer Master Plan, if a provisional sewer arrangement is allowed. Any consideration of a provisional sewer arrangement would also need to assess how ultimate sewer plans would be funded and implemented.

- d. Frontage and Street Improvements: Standard frontage improvements shall be designed and constructed when future subdivision and/or commercial site development processes are undertaken. The owner of the subject property north of Lions Park shall be responsible for right-of-way improvements along Meyer road from the northerly extent of the subject property to Highway 53, assuring appropriate roadway and walkway improvements are completed. No discontinuous improvements will be accepted.
  - e. Public Transit: Off highway bus stop areas shall be provided when the subject property is developed to assure future accessibility by public transit and school buses. Such areas shall be designed to provide safe and convenient staffing areas for riders so as not to conflict with vehicular and pedestrian traffic.
  - f. Well Lot: A well lot and necessary easements and rights-of-way in the Viking property will be dedicated to the City, if found appropriate, during the development review and platting process.
  - g. A buffer to the north along the property line include a fence that is not see-through or chain link, but some sort of vinyl and some sort of appropriate landscape boundary on the Viking side of the fence that will be approved by City staff, and
  - h. If City staff for any reason recommends a traffic study, Viking would then perform and provide such.
3. At approximately 49:30 Mr. Scott Krajack begins his rebuttal of public comment during which he states (at approximately minute 52:00) “we did offer, if it’s required by City staff, we will be willing to pay for a traffic impact analysis.”
  4. At approximately minute 55:30 Councilor Laws expresses her willingness to consider approval of the annexation in part based on the developer meeting with the property owners and working with them.
  5. At approximately minute 1:05:30 Councilor Worthen makes a motion to approve the annexation. Approval by the Council included consideration of the staff report and evidence presented at the hearing.

After discussion of these findings with the applicant, staff agreed to accept a level of service analysis as well as projected timing of their construction in relation to the Highway 53 / Meyer Road intersection improvements in lieu of the full traffic impact analysis at this time. This was supported by the City Engineer / Public Works Director. The applicant submitted a Level of Service Analysis Memo by Whipple Consulting Engineers, Inc. dated February 7, 2020. Based on the analysis it is anticipated that the level of service for the existing intersection will not be degraded until build-out of over 60 residential lots. The City has initiated a project for improvements at the intersection, to be paid for by Traffic Impact Fees as incorporated in the City’s Capital Facilities Plan, which is anticipated to be completed by mid-2022. Please see the Memo by City Engineer, Kevin Jump, attached within Exhibit B-7. Staff has included recommended conditions of approval to address project timing and traffic impacts, including the Idaho Transportation Department’s (ITD) requirement to perform a traffic impact analysis prior to connection of any new access points to Highway 53.

The applicant also submitted a revised plan showing a six-foot high sight obscuring fence on the north property line “to be installed south of the property line so as to ensure that the owner to the north has the ability to construct a second livestock fence if needed.” Staff has also recommended a condition of approval to address the applicant’s previous commitment to provide a three-foot buffer.

### **III. RECOMMENDATION**

In its review, the Planning and Zoning Commission determined the proposed subdivision conforms to the general purpose of the Comprehensive Plan and will include appropriate provisions for streets, drainage and other public ways, open space, water supplies, sewage disposal, fire protection and other appropriate public and private improvements upon infrastructure build-out. The City of Rathdrum Planning and Zoning Commission recommends APPROVAL of the Westwood Pines Preliminary Long Plat subject to the following conditions: Staff recommends the following findings of fact and conclusions of law for any approval of the proposed subdivision:

#### **Conditions of Approval:**

1. Issuance of building permitting within the subdivision, inclusive of all zone districts, will be limited to 60 dwelling units until capital improvements are completed for the SH53-Meyer Road intersection.
2. Any non-residential development within the subdivision, inclusive of all zone districts, will require a new, updated traffic LOS analysis and conformance with City requirements derived therefrom, as applicable.
3. A Traffic Impact Analysis shall be completed per the requirements of Idaho Transportation Department (ITD) prior to connection of Cloverdale Street access to SH53. Such Traffic Impact Analysis shall be submitted to the City as well as any response and/or requirements from ITD. The developer shall be responsible for providing any improvements identified as required by ITD.
4. The developer shall donate (at no cost to the City of Rathdrum and/or ITD), all necessary public right-of-way and easements on the north-side of SH53 and east-side of Meyer Road adjacent to the proposal which may be required to provide traffic improvements for the development.
5. The developer shall re-construct Meyer Road, from SH53 to the property's northwest boundary. The improvements for Meyer Road shall be in accordance with the City's collector street typical roadway standards, as approved by the City Engineer / Public Works Director.
6. The new, temporary sewer force main alignment shall be contained within noted / platted vehicle access aisles where it crosses the City's cemetery property. The new, permanent sewer force main alignment shall be contained within the Cloverdale Street public right-of-way. Both the temporary and permanent sewer force main alignments shall be constructed as part of this project. The Cloverdale Street force main alignment shall terminate on the north side of SH53.
7. The developer shall be responsible for constructing a 16-foot wide all-weather vehicle access aisle, in accordance with City requirements, within the sewer force main alignment within the cemetery property.
8. The developer shall construct a temporary sanitary sewer lift station with the Westwood Pines development, at the developer's sole expense. The City shall provide for the required wet well volume and technical specifications for: the lift station components, including electrical controls, bypass pumping capabilities, emergency power requirement, odor control equipment, telemetry, wet well access capabilities, spray-liner requirements for the wet well, site layout of the lift station and wet well liquid level sensor devices. Further, the City will provide direction for the sewer piping depth (invert elevation) and pipe sizing upstream of the temporary sewage lift station. City financial participation pursuant to RCC 9-4-10, Credits and Reimbursements, may be available if upsizing of the system is determined necessary. Lastly, this temporary sewer lift station shall be integrated in the City's SCADA monitoring system. The temporary lift station shall be placed within an easement as specified by the City Engineer / Public Works Director and shall be owned and operated by the City of Rathdrum upon acceptance by the City Council.

9. The developer shall construct a 12-inch diameter water main in both the Dayton Avenue public right-of-way alignment (entirety of alignment) and Cloverdale Street. The Cloverdale Street 12-inch water main shall terminate on the north side of SH53.
10. Construction of the temporary sewer force main to serve the development will bring the public sewer within 200-feet of the Lion's Club property which will require such property to connect to the public system and abandon their private system within one year of City acceptance of the temporary force main. The developer shall be required to provide a lateral stub to service the Lion's property and is encouraged to coordinate with the Lion's Club regarding timing and payment of their connection to the City's system, including capitalization fees. In no case shall connections for the RV park be allowed without written authorization of the Director. Pre-treatment or other provisions may be required.
11. The developer shall enter into a Development Agreement with the City which specifies property and term, project regulation and policies, conditions of approval (including improvements to be constructed, roadway drainage swales, landscaping / street trees, irrigation water service lines to roadway drainage swales and landscaped areas, maintenance of common area landscaping, fencing and roadway drainage swales, irrigation system casings, walkways and stormwater, street lights, streets, construction access, street closure, phasing, erosion sediment control plan, dedication of right of way, dedication of easements and sewer), improvement construction standards and procedures, performance guarantee, owner's warranty, and other City requirements as approved by the City Council.
12. The developer shall install a six-foot tall solid (wood or vinyl, with no gaps between slats) fence three-feet south of the northern boundary of the subdivision.

The developer shall comply with the rules and requirements of any agency with jurisdiction over the project, including those agencies which provided comment for this proposal, and all applicable laws, rules and regulations governing the project, whether specified herein or not.

**Findings of Fact:**

1. The applicant seeks approval of a preliminary long plat for development of a 113-lot subdivision, including 110 residential R-2S lots, 1 well lot, 1 commercial lot and 1 multifamily R-3 lot.
2. The applicant is the property owner, Viking Construction, Inc., represented by Ray Kimball of Whipple Consultant Engineers.
3. The project is located at NNA W Highway 53 / NNA N Meyer Road, generally at the northwest corner of Highway 53 and Meyer Road, to the north and east of the Lion's Club and ball fields.
4. The site is currently undeveloped.
5. The property was annexed into the Rathdrum city limits in 2011 and provided with R-2S, R-3 and C-2 zoning as found on the City's current zoning map and in compliance with the City's 2009 Future Land Use Map.
6. According to the Rathdrum City Code Section 12-3-1-1 every division of land for the purpose of sale, lease, or transfer or building development into five (5) or more lots, tracts or parcels within the incorporated limits of the City shall proceed in compliance with RCC Title 12.
7. The proposed lot sizes are:
  - a. In the Residential R-2S (low/medium density residential) zone district, 110 lots will range in size from 7,540 SF to 21,143 SF in area - with the average lot size being 8,995 SF
  - b. In the Residential R-3 (multifamily residential) zone district, 12 lot will be approximately 8.8 acres.

- c. In the commercial C-2 (highway / service commercial) zone district, one lot of approximately 11,025 SF is to be dedicated to the City of Rathdrum for the purpose of well placement, and the remaining approximately 10.8-acres is a commercial lot.
8. The minimum lot size required in the R-2S zone district is 7,500 square feet. The minimum lot size in the R-3 zone district is 6,000 square feet. There is no minimum lot size required in the C-2 zone.
9. The application is subject to processing pursuant to RCC 12-3-1-5 and 12-3-1-6.
10. An open record public hearing after due legal notice was held on January 15, 2020 before the Rathdrum Planning and Zoning Commission.
11. Appearing and testifying on behalf of the City of Rathdrum was City Planner, Cary Siess.
12. Appearing and testifying on behalf of the applicant was Ray Kimball of Whipple Consulting Engineers.
13. Public testimony from the following individuals was received:
  - a. Jim Dugan,
  - b. John Leitzke,
  - c. Sally Williams,
  - d. Richard Williams.
14. Traffic impacts and fencing of adjacent uses were concerns of the public and Planning and Zoning Commission expressed during the public hearing for this proposal.
15. Admitted into the record were comments from agencies as follows:
  - a. Idaho Transportation Department (ITD)
  - b. Idaho Department of Environmental Quality (DEQ)
  - c. Northern Lakes Fire Protection District
  - d. Kootenai County Community Development
  - e. United States Postal Service
  - f. Rathdrum City Engineer / Public Works Director 12/10/19 – Sewer Force Main Alignment
  - g. Rathdrum City Engineer / Public Works Director 2/12/20 – Traffic LOS Analysis
16. Admitted into the record were studies and information provided by the applicant as follows:
  - a. Whipple Consulting Engineers, Inc., Westwood Village Subdivision Highway 53 & Meyer Road, Rathdrum Trip Generation and Distribution Letter, April 18, 2019
  - b. Aquila Geosciences, Inc., Ground Penetrating Radar Survey at the Rathdrum Lion's Club Property at Rathdrum, Idaho, September 7, 1999
  - c. Whipple Consulting Engineers, Inc., LOS Analysis Memorandum, February 7, 2020
  - d. Scott Krajack, Viking Builders, LLC Land Development Director, House Construction Pace Email, February 7, 2020
17. The Planning and Zoning Commission made their recommendation following review of comments received and the facts of the proposal in compliance with RCC 12-3-1-7.
18. Any conclusion of law which is more appropriately a finding of fact is incorporated herein by this reference.

**Conclusions of Law:**

1. The property / project site is appropriately zoned for the proposal.
2. The application has been processed in compliance with RCC 12-3-1-1, 12-3-1-5 and 12-3-1-6.
3. Traffic related concerns have been appropriately addressed in the opinion of the City Engineer / PW Director.
4. The Applicant has agreed to install 6-foot sight obscuring fencing and leave a 3-foot buffer between such fencing and the northern property line of the subject property.
5. The proposal conforms, or can conform, with the application of conditions of approval, to the zone district regulations, the requirements of RCC Title 12 and is consistent with the Rathdrum Comprehensive Plan.
6. Any finding of fact which is more appropriately a conclusion of law is incorporated herein by this reference.

**Council Action:**

This Commission Report is being provided to the Council for consideration at the next regular meeting of the Council in compliance with RCC 12-3-1-8 (A). The Council shall review the report and all other relevant evidence and take action to approve, reject the recommendation, or return the plat to the Commission for modification.

Pursuant to RCC 12-3-1-8 (B), Council approval shall constitute authorization for the subdivider to develop detailed plans and specifications for the improvements and facilities of the subdivision and to prepare the final plat in strict accordance with the standards set forth by RCC Title 12 and any conditions imposed by the Council.

**Potential Motions:** The language set forth below is provided for guidance with no intent to suggest any specific action.

APPROVAL WITH RECOMMENDED FINDINGS, CONCLUSIONS AND CONDITIONS OF APPROVAL (Optional): I move to approve the Westwood Pines Preliminary Long Plat with the recommended Finding of Facts, Conditions of Law and Conditions of Approval as found within the Planning Commission’s recommendation and staff report dated March 4, 2020, finding that it is in accord with the City of Rathdrum Comprehensive Plan and Rathdrum City Code as conditioned.

ADDITIONAL CONDITIONS OF APPROVAL (Optional): I further move that the following additional conditions be met in accordance with Rathdrum City Code: \_\_\_\_\_

APPROVAL WITH NO CONDITIONS: I move to approve the Westwood Pines Preliminary Long Plat finding that it is in accord with the City of Rathdrum Comprehensive Plan and Rathdrum City Code.

DENIAL: I move to deny the Westwood Pines Preliminary Long Plat, finding that it is not in accord with Rathdrum City Code or the Rathdrum Comprehensive Plan as follows: \_\_\_\_\_ (cite section(s) and provide conclusion(s) of non-compliance)

The preliminary plat may also be returned to the Commission for modification pursuant to RCC 12-3-1-8.

**Staff Contact:**

For more information regarding this application, the complete project file, including application materials and project history, is available for review at Rathdrum City Hall at 8047 W. Main Street, Rathdrum, Idaho, or by contacting Planning staff:

Cary Siess, City Planner / Planning and Zoning Administrator

City of Rathdrum Public Works Department, Planning Division  
8047 W. Main Street, Rathdrum, Idaho 83858  
Phone: 208-687-2700 x 117  
Email: cary@rathdrum.org

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Attached Exhibits:

Exhibit A - Project Narrative

Exhibit B - Agency Comments

1. Idaho Transportation Department (ITD)
2. Idaho Department of Environmental Quality (DEQ)
3. Northern Lakes Fire Protection District
4. Kootenai County Community Development
5. United States Postal Service
6. Rathdrum City Engineer / Public Works Director 12/10/19 – Sewer Force Main Alignment
7. Rathdrum City Engineer / Public Works Director 2/12/20 – Traffic LOS Analysis

Exhibit C - Studies and Information

1. Whipple Consulting Engineers, Inc., Westwood Village Subdivision Highway 53 & Meyer Road, Rathdrum Trip Generation and Distribution Letter, April 18, 2019
2. Aquila Geosciences, Inc., Ground Penetrating Radar Survey at the Rathdrum Lion's Club Property at Rathdrum, Idaho, September 7, 1999
3. Whipple Consulting Engineers, Inc., LOS Analysis Memorandum, February 7, 2020
4. Scott Krajack, Viking Builders, LLC Land Development Director, House Construction Pace Email, February 7, 2020

Exhibit D - Preliminary Subdivision and Utilities Maps

1. Preliminary Plat – revised 1.30.20
2. Utility Plan 10.4.19
3. 12.12.19 Streets & Drainage Maps

Exhibit E - Rathdrum City Code Compliance Analysis Matrix

Exhibit F - Rathdrum Comprehensive Plan Analysis Matrix

Exhibit G – Annexation Agreement 2011

Exhibit H – Planning and Zoning Commission Meeting Minutes January 15, 2020

## Rathdrum City Code Analysis Matrix

Code	Consistent	Discussion
<b>Rathdrum City Code (RCC) Title 12, Subdivision</b>		
<b>RCC 12 Chapter 1, Definitions</b> contains definitions to be used in review of the application.	Yes	Such definitions were utilized in review of the proposal.
<b>RCC 12 Chapter 2, Short Plats</b> contains requirements and standards for short plats. The Chapter is generally not applicable to this application with the exception of 12-2-2, Exemptions.	NA	The proposal is not exempt.
<b>RCC 12 Chapter 3, Long Plats</b> contains requirements and standards for long plats.	Yes and NA	This chapter is provided by section below.
<b>12-3-1, Preliminary Plat</b>	“	
• <b>§12-3-1-1, Applicability</b>	Yes	The division is for the purpose of building development for more than five (5) lots.
• <b>§12-3-1-2, Exemptions</b>	NA	The proposal is not exempt as found in RCC 12-2-2.
• <b>§12-3-1-3, Preapplication Procedure</b>	Yes	After submission of the application on October 4, 2019 a preapplication meeting was held with the applicant and City staff on October 30, 2019.
• <b>§12-3-1-4, Application Fees</b>	Yes	The applicant submitted the required application, checklist, title report and fees.
• <b>§12-3-1-5, Plats and Plans</b> A. Requirements B. Adequacy of Plats and Plans C. Distribution of Plats and Plans	Yes	A. The Applicant submitted an application on October 4, 2019 showing the general layout of roads, lots, blocks and other elements of the subdivision. A hearing before the Planning and Zoning Commission was scheduled not less than 20 days after determining the application was complete and consistent with RCC 12-3-2, Preliminary Plat Standards, in compliance with this section. A pre-construction meeting will be required with the public works department prior to construction of any infrastructure necessary for this plat.  B. Staff reviewed the application materials and provided comments and/or red lined plans to the applicant on October 17, 2019 and October 30, 2019 during the preapplication meeting. The applicant resubmitted plans on November 7, 2019. Staff again reviewed the materials and issued a comment letter and/or red lined plans on December 11, 2019. The applicant resubmitted plans on December 12, 2019. Staff determined that plans were complete and substantially met the standards set forth in this title.  Staff provided a Notice of Public Hearing including a description of the proposal and invitation to provide

Code	Consistent	Discussion
		<p>comment to the following agencies on January 2, 2020: Lakeland School District, Northern Lakes Fire Department, Idaho Department of Environmental Quality, Panhandle Health District, Kootenai Metropolitan Planning Organization, Idaho Transportation Department, City of Rathdrum Police Department, City of Rathdrum Parks and Recreation Department and Avista. The Applicant also provided notice to agencies prior to application submittal.</p> <p><u>Comments Received:</u> Comments received are attached in Exhibit B.</p> <p>Idaho Transportation Department (ITD)</p> <p>Idaho Department of Environmental Quality (DEQ)</p> <p>Northern Lakes Fire Protection District</p> <p>Kootenai County Community Development</p> <p>United States Postal Service</p> <p>Rathdrum City Engineer / Public Works Director 12/10/19 – Sewer Force Main Alignment</p> <p>Rathdrum City Engineer / Public Works Director 2/12/20 – Traffic LOS Analysis</p>
<ul style="list-style-type: none"> <li>• <b>§12-3-1-6, Public Hearing</b></li> <li style="padding-left: 20px;"><b>A. Notice of Hearing</b></li> <li style="padding-left: 20px;"><b>B. Continuance of Hearing</b></li> </ul>	Yes	<p>A. Notice of the hearing, including time, place and summary of the proposal was posted in the Coeur d’Alene Press on December 24, 2019 in compliance with this section, and again on January 2, 2020 to correct a typographical error.</p> <p>Notice by regular mail to all property owners of land situated within three hundred feet of the external boundaries of the proposal was provided on January 2, 2020 in compliance with this section.</p> <p>A Public Notice was posted on the property on January 2, 2020 in compliance with this section. Notice was also posted on the City’s web site and notice board outside of City Hall on January 2, 2020. All notices included the date, time and place of the hearing as well as a summary of the proposal, and the description and location of the subject property.</p> <p>Furthermore, prior to the date of the public hearing, the City issued a written staff report dated January 10, 2020, integrating any public comments received regarding the application, and made available to the public a copy of the Staff Report for review and inspection. A copy of the staff report was provided to the Applicant or the Applicant’s designated representative and the Planning Commission prior to the hearing.</p> <p>Staff also provided letters to those members of the public</p>

Code	Consistent	Discussion
		<p>who attended the hearing and requested information regarding continuance on February 11, 2020 as a courtesy – see below.</p> <p>B. A public hearing was held before the Rathdrum Planning and Zoning Commission on January 15, 2020. The Commission took public testimony as found herein. During deliberations the Commission requested more information from the applicant in relation to traffic and fencing and continued deliberations to the next regular meeting to be held on February 19, 2020.</p>
<ul style="list-style-type: none"> <li>• <b>§12-3-1-7, Commission Action</b></li> </ul>	Yes	<p>Following the public hearing, at the next regular meeting on February 19, 2020, the Commission considered all relevant evidence and comments to determine whether to recommend that the Council approve or disapprove the preliminary plat or return the preliminary plat to the subdivider for modification. The Commission determined that the proposed subdivision conforms to the general purpose of the Comprehensive Land Use Plan and includes appropriate provisions for streets, drainage, alleys and other public ways, open space, water supplies, sewage disposal, fire protection and other appropriate public and private improvements as required by Code. The Commission voted to recommend approval of the proposal, subject to conditions of approval as delineated herein.</p>
<ul style="list-style-type: none"> <li>• <b>§12-3-1-8, Council Action</b></li> </ul>	Unknown	<p>The Council shall review the application following receiving the Commission’s report and other relevant evidence and take action to approve, reject, or return the plat to the commission for modification.</p>
<ul style="list-style-type: none"> <li>• <b>§12-3-1-9, Expiration of Approval</b></li> </ul>	NA	<p>This section is not applicable until approval of the application.</p>
<p><i>12-3-2, Preliminary Plat Standards</i></p>	Yes	<p>The application contains the standards as numbered A through S of this section.</p>
<p><i>12-3-3, Final Plat</i></p>	NA	<p>Not applicable at this time.</p>
<p><i>12-3-4, Final Plat Standards</i></p>	NA	<p>Not applicable at this time.</p>
<p><b>RCC 12, Chapter 4, Design and Improvement Standards</b> contains street, block and lot standards for subdivisions, and construction plan specifications and standards.</p>	Yes	<p>Streets, blocks and lots have been determined to be compliant or in keeping with existing approved subdivisions and development.</p> <p>Construction plans and specifications will be reviewed for compliance at the time of submittal of such in the future (following preliminary plat approval).</p>
<p><b>RCC 12, Chapter 5, Municipal Land Donation</b> involves municipal land donation.</p>	NA	<p>The City has adopted impact fees which are applicable to the project and preempt this chapter.</p>
<p><b>RCC 12, Chapter 6, Variances</b> deals with Variances for this</p>	NA	<p>No request for variance has been requested.</p>

<b>Code</b>	<b>Consistent</b>	<b>Discussion</b>
Title.		
<b>RCC 12, Chapter 7, Violations and Enforcement</b> deals with Violations of this Title.	NA	No violations have been found.
<b>RCC 12, Chapter 8, Amendments</b> deals with Amendments to this Title	NA	Not applicable to the application.
<b>RCC Title 7, Public Ways and Property</b>		
<b>RCC 7, Chapter 1, Public Works Projects, Performance Bonds</b> deals with requirements for anyone engaging in municipal construction projects such as water and sewer main extension	Yes As Conditioned	Construction of infrastructure to be dedicated to the public will be required to conform with the requirements of this chapter, including licensing and bonding. The City Council shall approve any infrastructure prior to acceptance.
<b>RCC 7, Chapter 7, Right of Way Encroachment</b> deals with requirements for working in City right of way	Yes As Conditioned	The developer will be required to obtain permitting from the City and provide any required sureties prior to work within right-of-way for this development.
<b>RCC Title 8, Water and Sewer</b>		
<b>RCC 8, Chapter 1, Water System</b> deals with metering and fees for users of the municipal water system	Yes As Conditioned	The development of lots within the subdivision will be subject to the requirements of this chapter.
<b>RCC 8, Chapter 2, Cross-Connection Control</b> deals with protecting public health by controlling actual or potential cross-connections and contamination of the public water system.	Yes As Conditioned	The development of lots within the subdivision will be subject to the requirements of this chapter.
<b>RCC 8, Chapter 3, Wastewater Collection and Disposal</b> deals with the regulation of wastewater disposal, construction, ownership, connections, line extension policies, permitting, user charges, fees, easements, etc.	Yes, As Conditioned	Connection to the City's municipal sewer system is required. The developer is responsible to provide services necessary to serve the subdivision and dedicate infrastructure to the public upon acceptance by the City Council. Construction of infrastructure to be dedicated will be required to conform with the requirements of this chapter and shall be reviewed at the time of construction plan submittal.
<b>RCC 8, Chapter 4, Wastewater Discharge Restrictions</b> defines prohibited wastes to the municipal sewer system, including but not limited to RV waste.	Yes As Conditioned	Typical domestic waste is not generally applicable to this chapter, however, restriction of septic waste, including from RVs is. As conditioned the project will be compliant.
<b>RCC Title 9, Building Regulations</b>		

Code	Consistent	Discussion
<b>RCC 9, Chapter 1, Building Codes</b> defines adopted international codes, and contains requirements for building permitting and fees	Yes As Conditioned	Building on individual lots will be subject to review under the applicable international codes and permitting by the City at the time of development (following final plat approval).
<b>RCC 9, Chapter 2, Mechanical Code and Fuel Gas Installers</b> defines adopted international codes, and contains requirements for permitting and fees	Yes As Conditioned	Building on individual lots will be subject to review under the applicable international codes and permitting by the City at the time of development (following final plat approval).
<b>RCC 9, Chapter 4, Development Impact Fee</b> the purpose of this chapter is to prescribe the procedure whereby developers of land shall pay an impact fee for the purpose of providing the public facilities and system improvements needed to serve future residents and users of such development, ensure adequate facilities are provided to serve new growth, ensure those who benefit from the growth and development pay their proportionate share of new public facilities needed to serve the growth and development, collect and expend development impact fees and ensure capital improvements (which are approved and listed in the City's Capital Improvement Plan) are fully funded by the fees. The chapter deals with calculating fees, collection, etc.	Yes As Conditioned	Development impact fees are paid and collected at the time of building permitting and calculated based on the use. The capital improvements to be financed by the impact fee are those listed in the City's adopted Capital Improvement Plan  This chapter shall not affect, in any manner, the permissible use of property, density of development, design and improvement standards and requirements, or any other aspect of the development of land or provision of capital improvements subject to the zoning and subdivision regulations or other regulations of the city, which shall be operative and remain in full force and effect without limitation with respect to all such development.
<b>RCC Title 11, Zoning</b>		
<b>RCC 11, Chapter 4, Zoning Districts</b> defines zoning districts and boundaries as found within the adopted City Zoning Map and Future Land Use Map	Yes	The zoning districts within the subdivision were established by amendment of the City Zoning Map by the City Council at the time of annexation of the property. The proposed development follows the adopted boundaries within the approved R-2S, R-3 and C-2 districts.
<b>RCC 11, Chapter 4, Zoning, Article A-2-1, R-2S Residential District</b> provides for establishment of single family	Yes	The proposed lot configuration within the R-2S zone district provides for establishment of a single-family neighborhood with sufficient area to provide for minimum lot sizes, setbacks and appropriately sized building envelopes when such standards

Code	Consistent	Discussion
neighborhood development subject to the established lot area, density, setback and other provisions of the chapter.		are applied at the time of building development.
<b>RCC 11, Chapter 4, Zoning, Article A-4, R-3 Residential District</b> provides for residential environments at a higher density than other residential districts, commonly known as the Multifamily Zone District, allowing apartments, townhouses, condos, schools, churches and other uses subject to the established lot area, density, setback and other provisions of the chapter.	Yes As Conditioned	There is currently no proposal for development of the established R-3 zone within the development, only segregation of the zoning from adjacent zone districts as established at the time of annexation of the property by the City Council. The adopted standards will be applied at the time of development of the lot, allowing up to approximately 252 residential units or other permitted or conditional uses in the future, subject to permitting.
<b>RCC 11, Chapter 4, Zoning, Article B-3, C-2 Highway / Service Commercial</b> provides area for highway and service related businesses on or near main arterials as defined within the permitted and conditionally permitted uses, subject to the established density, setback and other provisions of the chapter.	Yes As Conditioned	There is currently no proposal for development of the established C-2 zone within the development, only segregation of the zoning from adjacent zone districts as established at the time of annexation of the property by the City Council. The adopted standards will be applied at the time of development of the lot in the future, subject to permitting.
<b>RCC 11, Chapter 5, General Provisions and Performance Standards, Section 1, General Standards</b> provides for required right-of-way / frontage improvements required at the time of land development, including driveway approaches, sidewalks, fencing, stormwater management, and other provisions.	Yes As Conditioned	The proposal has been reviewed and determined to provide adequate lot frontages to accommodate driveways, including separation between approaches, areas for stormwater retention, sidewalks and other frontage improvements. As conditioned, the developer will be required to enter into a Development Agreement, as approved by the City Council, which will specify project regulation and policies, conditions of approval (including improvements to be constructed, roadway drainage swales, landscaping / street trees, irrigation water service lines to roadway drainage swales and landscaped areas, maintenance of common area landscaping, fencing and roadway drainage swales, irrigation system casings, walkways and stormwater, street lights, streets, construction access, street closure, phasing, erosion sediment control plan, dedication of right of way, dedication of easements and sewer), improvement construction standards and procedures, performance guarantee, owner's warranty, and other City requirements in conformance with this chapter. Construction plans and specifications will be reviewed for compliance at the time of submittal of such in the future.

Code	Consistent	Discussion
<p><b>RCC 11. Chapter 5, General Provisions and Performance Standards, Section 2, Residential Standards</b> provides standards for landscaping, driveways and parking, garages, accessory buildings and other provisions for residential development.</p>	<p>Yes As Conditioned</p>	<p>Plans and specifications will be reviewed for compliance at the time of submittal of individual lot development in the future.</p>
<p><b>RCC 11. Chapter 5, General Provisions and Performance Standards, Section 3, Commercial, Industrial, Multi-Family and Institutional Standards</b> provides standards for landscaping, parking, loading, lighting, trash areas, material storage and other provisions for development.</p>	<p>Yes As Conditioned</p>	<p>Plans and specifications will be reviewed for compliance at the time of submittal of individual lot development in the future.</p>
<p><b>RCC 11, Chapter 5, General Provisions and Performance Standards, Section 4, Site Plan Review</b> provides standards to ensure commercial, industrial, multi-family and institutional development are in compliance with zoning and other City Code requirements and/or policy as well as other regulations.</p>	<p>Yes As Conditioned</p>	<p>Plans and specifications will be reviewed for compliance at the time of submittal of individual lot development in the future.</p>
<p><b>RCC 11, Chapter 12, Development / Annexation Agreements</b> provides for the opportunity for the City to require an agreement, the form of which is required by the Zoning Administrator.</p>	<p>Yes As Conditioned</p>	<p>Staff has reviewed the Annexation Agreement for the property and determined that the proposal is in keeping with the terms and conditions therein. As conditioned, the developer shall enter into a Development Agreement with the City, as approved by the City Council to address specific project regulation and policies, conditions of approval (including improvements to be constructed, roadway drainage swales, landscaping / street trees, irrigation water service lines to roadway drainage swales and landscaped areas, maintenance of common area landscaping, fencing and roadway drainage swales, irrigation system casings, walkways and stormwater, street lights, streets, construction access, street closure, phasing, erosion sediment control plan, dedication of right of way, dedication of easements and sewer), improvement construction standards and procedures, performance guarantee, owner's warranty, and other City requirements in conformance with City Code and policy.</p>

<b>Code</b>	<b>Consistent</b>	<b>Discussion</b>
RCC 11, Chapter 16, Signs provides for sign standards and requirements.	Yes As Conditioned	Plans and specifications will be reviewed for compliance at the time of submittal of individual lot development in the future.

**Comprehensive Plan Analysis Matrix**

Goal/Policy	Consistent	Discussion
<b>II A. Population</b>		
<b>II A. Population, Goal 1: Address changing growth trends and capacities to work toward a moderate and stable rate of long-range population growth with orderly development.</b>	Yes	The proposal addresses long-range population growth within a subdivision development which is consistent with other approved development in Rathdrum.
<b>II A. Population, Goal 2: Guide future development to maintain and enhance the quality of life of the population living within and directly adjacent to the community.</b>	Yes As Conditioned	The proposal can address quality of life of the property owners located to the north, adjacent to Rathdrum, with inclusion of the recommended conditions of approval.
<b>II B. Land Use</b>		
<b>II B. Land Use, Goal 1: A balance of land use guidelines will be applied to preserve and enhance the following values:</b> <ul style="list-style-type: none"> <li>• Rural character and small-town feel</li> <li>• Access to open space</li> <li>• Provision of day-to-day commercial needs</li> <li>• Well maintained and inter-connected local circulation systems</li> <li>• Economically rational development of desirable land uses</li> <li>• Historic sites</li> <li>• Capacity for citizens to live, work and play inside the community</li> <li>• Public safety</li> <li>• Quality of the local natural environment</li> <li>• Preservation of a clean and abundant aquifer</li> <li>• Efficient use of valuable infrastructure.</li> </ul>	Yes As Conditioned	The proposal is consistent with other development within Rathdrum. A portion of the proposal includes segregation of the commercially zoned property from the residentially zoned property and will provide capacity for provision of day to day commercial needs. The proposed subdivision will provide inter-connectivity with existing streets and provide capacity for citizens to live in the community.
<i>II B. Land Use, Goal 1, Policy A: Actively engage citizens in planning and zoning activities to ensure desirable land use practices are applied and sustained.</i>	Yes As Conditioned	The City provided public notice of the proposal as required by law and encouraged citizens to engage in the subdivision process. No comments in opposition of the proposed subdivision itself were received. As conditioned, fencing and buffering which is desirable to the public will be required.
<i>II B. Land Use, Goal 1, Policy E: Apply land use regulations in a manner respecting overall property values while addressing other specified community values.</i>	Yes As Conditioned	As conditioned, fencing and buffering which is desirable to the public will be required.
<b>II E. Public Services</b>		
<i>II E. Public Services, Goal 1, Policy A: Coordinate with service providers and public safety entities to assure necessary and appropriate</i>	Yes	Service providers and public safety entities were provided an opportunity to comment on the proposal as required. Such providers can

<b>Goal/Policy</b>	<b>Consistent</b>	<b>Discussion</b>
<i>urban services are provided to accommodate projected types and densities of growth and development.</i>		accommodate the proposal as found within the comment letters provided or by lack of comment.
<b>II E.-1 Public Safety, Goal 1, Policy E:</b> <i>Continue to coordinate with fire protection officials to ensure that new development is designed so fire protection can be effectively provided.</i>	Yes	Northern Lakes Fire Protection District was given an opportunity to provide comment on the proposal and provided response that fire protection can be provided.
<b>II E.-1 Public Safety, Goal 1, Policy F:</b> <i>Require new development to provide sufficient emergency vehicular access for emergency equipment and adequate water supply and pressure for the purposes of fire protection.</i>	Yes	The proposed subdivision will be required to meet these provisions prior to recordation of plat or building permitting.
<b>II E.-1 Public Safety, Goal 1, Policy G:</b> <i>Require development be designed and constructed to address the minimization of hazards and the enhancement of the effective provision of law enforcement and protection.</i>	Yes As Conditioned	As conditioned, fencing and buffering to address possible hazards to the north (livestock) will be required.
<b>II E.-2 Sewer and Water</b>		
<b>II E.-2 Water and Sewer, Goal 1, Policy A:</b> <i>Require all existing and proposed facilities within the city limits be connected to and serviced by, the municipal sewer system.</i>	Yes	Connection to the City sewer system is required.
<b>II E.-2 Water and Sewer, Goal 1, Policy B:</b> <i>Promote development in areas with municipal sewer and water systems in place.</i>	Yes	Municipal sewer and water lines are located adjacent to the proposal and can be extended by the developer to provide service for the proposal.
<b>II F. Transportation</b>		
<b>II F. Transportation, Goal 1, Policy A:</b> <i>Promote standards that create functional connections to the regional highway system and convenient internal circulation.</i>	Yes As Conditioned	The proposal includes provision of convenient internal circulation. As conditioned, functional connection to Highway 53 will be implemented.
<b>II F. Transportation, Goal 1, Policy D:</b> <i>Incorporate lighting, sidewalks and connections to enhance neighborhoods, pedestrian and bike pathways, public safety, etc.</i>	Yes	The proposal includes the provision of lighting and sidewalks.
<b>II F. Transportation, Goal 2, Policy H:</b> <i>Explore alternative road and street designs to enhance and preserve natural and aesthetic characteristics of the community.</i>	Yes	The City Engineer has approved an alternate street design for Meyer Road to accommodate narrower right of way for protection of the City Cemetery and possible cultural site.
<b>II G. Housing</b>		
<b>II G. Housing, Goal 1: Apply four primary principles, 1) Sense of Community, 2) Variety, 3) Affordability and 4) Integration, in the</b>	Yes	The proposal includes lighting and sidewalks to encourage walking and cycling and will provide increased housing stock which helps

Goal/Policy	Consistent	Discussion
<p>enhancement and maintenance of existing residential areas and the design of new housing developments.</p> <p><b>A) Sense of Community:</b> Promote a sense of community by encouraging walking and cycling, good lighting and safe havens for children while providing ready access to shared green space and parks. Housing will be connected to commercial areas and civic centers by well-organized street and pathway systems, potentially including public transportation, all making it easy for people to meet and interact with each other. Neighborhoods should not be completely walled off from each other.</p> <p><b>B) Variety:</b> Provide a broad mix of housing choices for a mix of people with differing economic means, ages and lifestyles. Types of housing may include, but are not limited to, apartments, townhouses, multi-family units, manufactured homes and small, as well as large, lot single family residences.</p> <p><b>C) Affordability:</b> Ensure affordable housing is available for the local household and family, encouraging the creation of a wide range of housing, including that which is affordable to the community's average and lower income worker and family.</p> <p><b>D) Integration:</b> Provide a mix of appropriate housing choices in neighborhoods suitable for seniors as well as youth, young families starting out, as well as established single-family residences. Neighborhoods should be welcoming, inviting, safe and available in every sense. Seniors should not be segregated from youth. Young families starting out should not be segregated in apartment complexes from single family residential neighborhoods.</p>		<p>keep the market affordable.</p>
<p><i>II G. Housing, Goal 2, Policy C: Enforce buffering requirements for non-residential activities developed adjacent to residential areas.</i></p>	<p>Yes As Conditioned</p>	<p>As conditioned, fencing and buffering along the northern property boundary will separate the residential subdivision from Agriculturally zoned property.</p>
<p><i>II G. Housing, Goal 2, Policy F: Protect the continuity and inherent character of existing neighborhoods in planning and zoning decisions.</i></p>	<p>Yes As Conditioned</p>	<p>As conditioned, fencing and buffering will help preserve the character of the properties to the north of the development.</p>
<p><b>II G. Housing, Goal 5: Accommodate the enhancement of current housing and the</b></p>	<p>Yes</p>	<p>Increased housing stock helps keep the market</p>

Goal/Policy	Consistent	Discussion
development of future housing to ensure housing is available in an affordable manner for all members of the community.		affordable.
<b>II K.-1 Small Town Atmosphere</b>		
<i>II K.-1 Community Design, Small Town Atmosphere, Goal 1, Policy A: Implement community development tools and land use regulations to advance the community's sense of place and small town character.</i>	Yes	The development standards as found within Rathdrum City Code, which was developed in light of the Comprehensive Plan, have been utilized in review of the project.
<i>II K.-1 Community Design, Small Town Atmosphere, Goal 1, Policy C: Consider building height and bulk standards for individual districts to minimize impacts on views from public spaces</i>	Yes	Building height and bulk standards will be applied at the time of lot development in conformance with Code.
<i>II K.-1 Community Design, Small Town Atmosphere, Goal 1, Policy D: Encourage preservation, protection and enhancement of historic properties in respect of city heritage.</i>	Yes	The proposal incorporates a modified road section in order to protect historic burial sites.
<b>II K.-2 Community Design, Balanced Development</b>		
<i>II K.-2 Community Design, Balanced Development, Goal 2, Policy D: Encourage residential development within commercial nodes.</i>	Yes	The proposed residential development is within an existing commercial node adjacent to Highway 53.



### Westwood Pines Subdivision

The site of the proposed annexation is located in the Southwest  $\frac{1}{4}$  of Section 29, T52N, R4W near the intersection of Meyer Road and Highway 53. See below for a Vicinity Map.

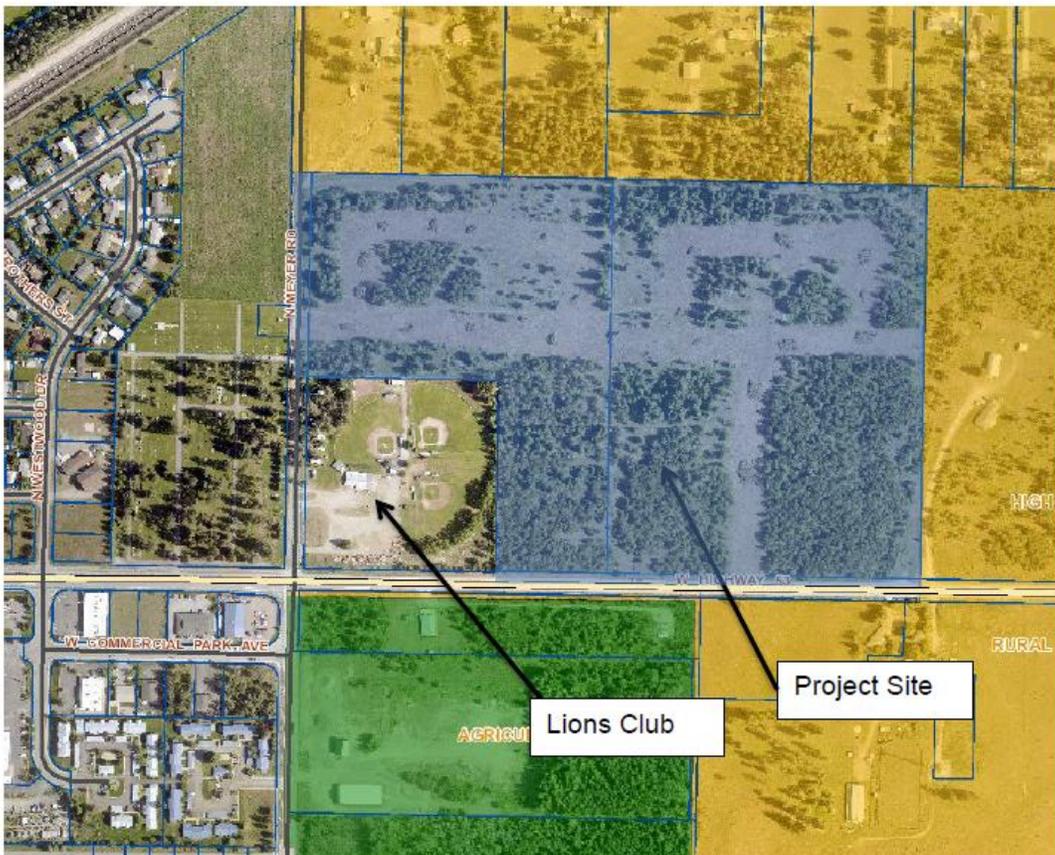


Figure 1: Vicinity Map

The property consists of two vacant parcels, totaling approximately 50 acres. It is located immediately north and east of the Lions Club property and has frontage on both Highway 53 and on Meyer Road. The site is generally forested with new growth pines and is considered flat/rolling, with approximately 20 feet of relief across the 50 acre site. As such, there are no physical limitations that would prohibit land development as proposed. This property was annexed into the City of Rathdrum in 2011 with three zoning designations. The frontage along Highway 53 was designated C-2 and R-3, while the northern portion was zoned R-2S. (See below zoning map.) The purpose of this application is to segregate the three zones as well as to

subdivide the R-2S property in conformance with the requirements of that zone and the City's subdivision ordinance.

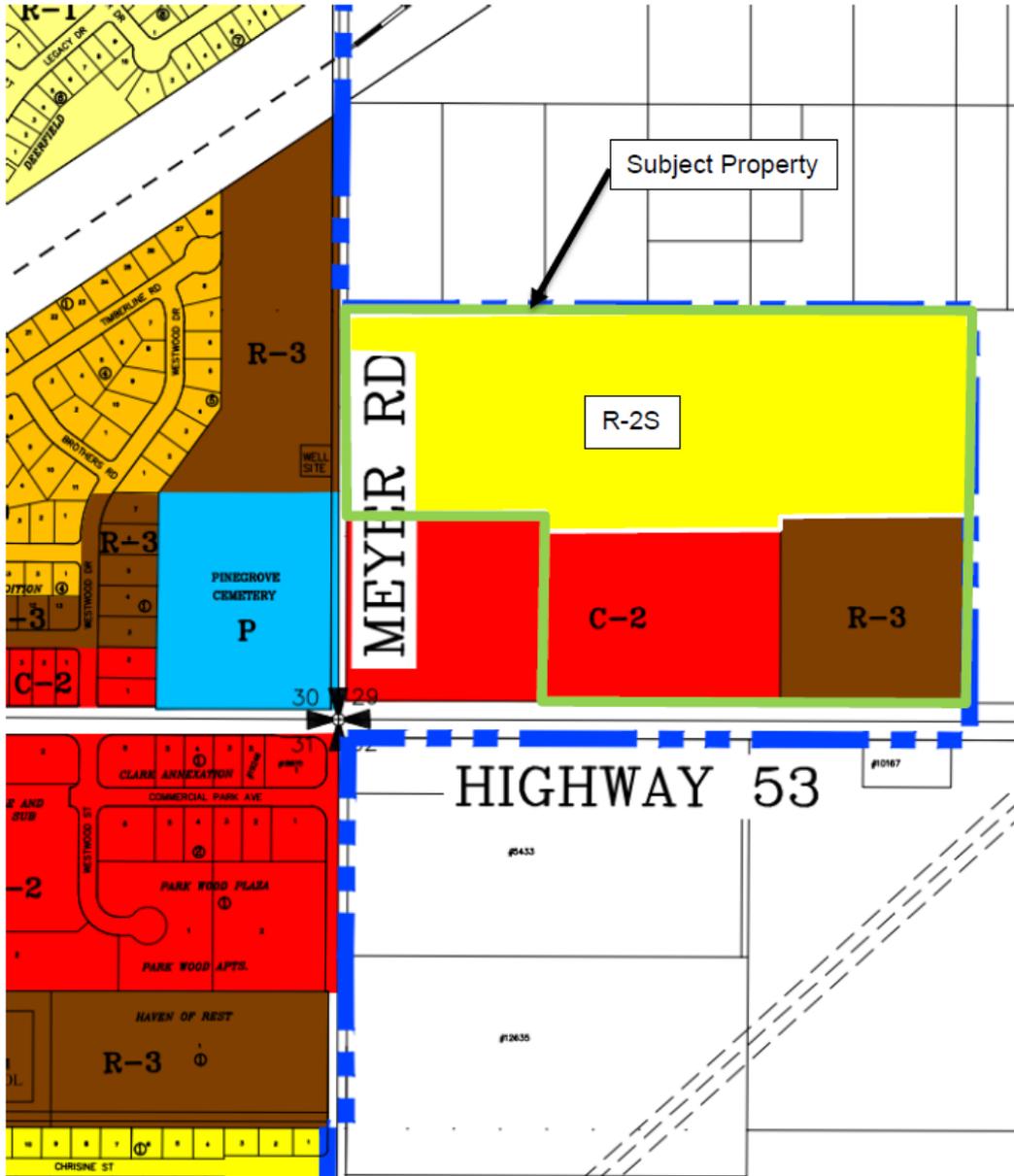


Figure 2: Current Zoning Map

As shown in the above two maps, the adjacent zoning is County Rural to the north, east, and south. There is also some County Agricultural zone to the south across the highway. To the west is R-3 zoning of the City's property, C-2 zoning of the Lions Club property, and P zoning for the Pinegrove Cemetery.

**Subdivision:**

As shown on the attached subdivision plan, the proposed subdivision will result in 110 single family residential lots, one commercial (C-2) lot located to the west of Cloverdale Street, and one R-3 lot located to the east of Cloverdale Street. The lots range in size from a minimum of 7,540 sf to a maximum of 21,143 sf with the average size in the neighborhood of 8,995 sf. All lots exceed the minimum requirements of the zone in which they are located. It is anticipated that the subdivision will be constructed in three phases as shown on the subdivision plan drawings. The first phase will include 41 single family lots as well as segregating the C-2 and the R-3 zoned parcels. Phase two will include the 34 single family lots on Astoria Loop. Phase three will consist of the 35 single family lots on Pelican Loop.

Meyer Road will be improved from Highway 53 north to the northern boundary of the project. South of Highway 53, Meyer Road is considered a Collector, but north of the Highway, it functions as a local street due to the fact that it is cut off by the railroad. It's also important to note that the width of Meyer road near the Highway is limited by several factors, the most notable of which is the existence of the unmarked Chinese railroad worker graves on the east side of the right of way on and adjacent to the Lions club property. As such, we propose that Meyer road be limited to 36' curb to curb width, which allows for three 12' lanes (Northbound Through, Southbound Left, Southbound Through/Right) and that the alignment crowd to the west so as to minimize the potential to disturb any of the unmarked graves. As Meyer extends north past the end of the southbound left turn pocket, we propose to construct the east half of Meyer plus the western part needed to provide safe travel to and across the frontage of the subject property.

The internal streets will be built to the City's local street standard and are designed such that they provide a great neighborhood feel while still promoting proper circulation and connectivity. The extension of Dayton Avenue to the east will provide important connectivity to the adjacent undeveloped parcel and will allow for them to develop in the future should the land owners decide to do so. Cloverdale will function as the secondary access to the property, thus meeting the requirements of the Fire Code for access. Whereas Meyer is expected to serve as the primary residential access to this subdivision, Cloverdale will serve as the primary access for both the C-2 and the R-3 zoned parcels. The approach onto Highway 53 at Cloverdale will be subject to the ITD permitting process, which may limit access to right in/right out or may require highway widening to allow for turn pockets. ITD has requested that a traffic study be completed for the proposed intersection so as to ensure that the new intersection does not cause unmitigated operational or safety issues for the traveling public. This traffic

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study would be expected to be a condition of approval for the subdivision. The intersection of Meyer Road and Highway 53 is scheduled for signalization in the very near future, the design and construction of which will greatly improve the function of that intersection.

An existing 12" diameter water main is located within Meyer Road and will serve as the water source for this subdivision. Gravity sewer is not available due to the fact that the property lies within an undeveloped sewer basin. As such, gravity sewer will be constructed to a new public lift station which will be constructed at the low point of the property. That lift station will pump to an existing manhole located in the cul-de-sac of Timberline Street. Stormwater will be collected in street side swales for treatment and discharge into drywells. Dry utilities are also located on the property adjacent to both Highway 53 and Meyer, and they are available to serve any future development on site.

**Conclusion:**

The proposed subdivision is supported by the comprehensive plan and the proposed subdivision meets the requirements of the City's subdivision ordinance; therefore, we are requesting approval of the subdivision of the property as presented.

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**IDAHO TRANSPORTATION DEPARTMENT**  
600 W. Prairie Ave.  
Coeur d'Alene, ID 83815-8764

(208) 772-1200  
[itd.idaho.gov](http://itd.idaho.gov)

September 12, 2019

Whipple Consulting Engineers  
Attn: Ray Kimball  
21 S. Pines Road  
Spokane Valley, WA 99206

Re: **Westwood Pines- Prelim Plat**

Dear Mr. Kimball:

Upon review of the submitted Preliminary Plat for the Westwood Pines Subdivision, the Department will require a Traffic Impact Study pursuant to Idaho Administrative Code IDAPA 39.03.42. This is to ensure that the additional access will not cause operational or safety problems for the traveling public. A Traffic Impact Study shall include a comprehensive analysis of the anticipated transportation network conditions with and without an applicant's proposed new or modified access, including an analysis of mitigation measures. The applicant will be required to submit and complete a permit application for an approach to State Highway 53 as indicated in the preliminary plat.

If you have any further questions concerning this request, feel free to contact our Permit Coordinator Stacy Simkins at 208-772-1297.

Sincerely,

A handwritten signature in blue ink that reads 'Nathan Herbst'.

Nathan Herbst, P.E.  
Traffic Engineer



State of Idaho  
Department of  
Environmental Quality

2110 Ironwood Parkway • Coeur d'Alene, ID 83814 • (208) 769-1422  
www.deq.idaho.gov

Brad Little, Governor  
John H. Tippetts, Director

## DEQ Response to Request for Environmental Comment

Date:	September 10, 2019
Agency Requesting Comments:	Viking Construction/Whipple Consulting Engineers
Date Request Received:	8/27/2019
Applicant/Description:	Westwood Pines proposed 110 lot single family residential subdivision application. This subdivision will also result in two remainder lots which have an existing zoning of C-2 and R-3 respectively.

Thank you for the opportunity to respond to your request for comment. While the Idaho Department of Environmental Quality (DEQ) does not review projects on a project-specific basis, we attempt to provide the best review of the information provided. DEQ encourages agencies to review and utilize the Idaho Environmental Guide to assist in addressing project-specific conditions that may apply. This guide can be found at <https://www.deq.idaho.gov/assistance-resources/environmental-guide-for-local-govts/>

DEQ has not completed a thorough review of the documents provided, therefore, the following general comments should be applied as appropriate to the specific project:

### 1. Air Quality

- The City should consider requiring reasonable controls on fugitive dust emitting activity during all phases of the project (including but not limited to; roadway construction, vehicle traffic on unpaved roads, land clearing activity, topsoil management, vegetation management, infrastructure installation). All reasonable precautions shall be taken to prevent particulate matter (dust) from becoming airborne, as required in IDAPA 58.01.01.651.

If open burning of land clearing debris is incorporated into the land clearing phase, smoke management practices to protect air quality as described in IDAPA 20.02.01.071.03 and IDAPA 58.01.01.614 must be implemented by the applicant. Local fire protection permits may also be required. The City should consider requiring smoke management actions be developed if open burning occurs during the land clearing phase.

The City should consider requiring a project plan that commits the developer to the proper disposal of demolition and construction debris. Open burning of demolition or construction debris is not an allowable form of open burning as defined by IDAPA 58.01.01.600. Demolition and construction debris must be treated in accordance with solid waste regulations.

- IDAPA 58.01.01.201 requires an owner or operator of a facility to obtain an air quality permit to construct prior to the commencement of construction or modification of any facility that will be a source of air pollution in quantities above established levels. DEQ asks that cities and counties require a proposed facility to contact DEQ for an applicability determination on their proposal to ensure they remain in compliance with the rules.

For questions, contact the DEQ Air Quality Permitting Hotline at 1-877-573-7648.

## **2. Wastewater**

- DEQ recommends that projects be served by existing approved wastewater collection systems or a centralized community wastewater system whenever possible. Please contact DEQ to discuss potential for development of a community treatment system along with best management practices for communities to protect ground water.
- If connecting to an existing wastewater utility, DEQ recommends verifying that there is adequate capacity to serve this project prior to approval. Please contact the sewer provider for a will-serve letter stating the provider's capacity to serve the project, willingness to serve this project, and a declining balance of available connections.
- IDAPA 58.01.16 and IDAPA 58.01.17 are the sections of Idaho rules regarding wastewater and recycled water. Please review these rules to determine whether this or future projects will require DEQ approval. IDAPA 58.01.03 is the section of Idaho rules regarding subsurface disposal of wastewater. Please review this rule to determine whether this or future projects will require permitting by the district health department.
- All projects for construction or modification of wastewater systems require preconstruction approval. Recycled water projects and subsurface disposal projects require separate permits as well.

For questions, contact Matt Plaisted, DEQ Water Quality Engineering Manager, at (208)769-1422.

## **3. Drinking Water**

- DEQ recommends using an existing drinking water system whenever possible or construction of a new drinking water system. Please contact DEQ to discuss this project and to explore options to best serve the future residents of this development and provide for protection of ground water resources.
  - IDAPA 58.01.08 is the section of Idaho rules regarding public drinking water systems. Please review these rules to determine whether this or future projects will require DEQ approval. All projects for construction or modification of public drinking water systems require preconstruction approval.
  - If any private wells will be included in the proposed project, DEQ recommends at a minimum testing the private well for total coliform bacteria, nitrate, and nitrite prior to use and retested
-

annually thereafter.

For questions, contact Matt Plaisted, DEQ Water Quality Engineering Manager, at (208) 769-1422.

#### **4. Surface Water**

- If the proposed project will involve de-watering of ground water during excavation and discharge back into surface water, a short-term activity exemption (STAE) from DEQ is required, including a description of the water treatment from this process to prevent excessive sediment and turbidity from entering surface water.
- Please contact DEQ to determine whether this project will require an Idaho Pollution Discharge Elimination System (IPDES) Permit for a discharge to state waters. If this project disturbs more than one acre, a stormwater permit from EPA may be required. Other construction general permits issued by EPA may be applicable as well.
- If this project is near a source of surface water, DEQ requests that projects incorporate construction best management practices (BMPs) to assist in the protection of Idaho's water resources. Additionally, please contact DEQ to identify BMP alternatives and to determine whether this project is in an area with Total Maximum Daily Load stormwater permit conditions.
- The Idaho Stream Channel Protection Act requires a permit for most stream channel alterations. Please contact the Idaho Department of Water Resources (IDWR), Northern Regional Office, at 7600 N. Mineral Drive, Suite 100, Coeur d'Alene, or call 208-762-2800 for more information. Information is also available on the IDWR website at: <http://www.idwr.idaho.gov/WaterManagement/StreamsDams/Streams/AlterationPermit/AlterationPermit.htm>
- The Federal Clean Water Act requires a permit for filling or dredging in waters of the United States. Please contact the US Army Corps of Engineers, Coeur d'Alene Field Office, at 1910 Northwest Blvd, Suite 210, Coeur d'Alene, ID 83814, or call 208-433-4474 for more information regarding permits.

For other questions about surface water quality, contact Tom Herron, DEQ Surface Water Manager, at (208) 769-1422.

#### **5. Solid/Hazardous Waste And Ground Water Contamination**

- **Hazardous Waste.** The types and number of requirements that must be complied with under the federal Resource Conservation and Recovery Act (RCRA) and the Idaho Rules and Standards for Hazardous Waste (IDAPA 58.01.05) are based on the quantity and type of waste generated. Every business in Idaho is required to track the volume of waste generated, determine whether each type of waste is hazardous, and ensure that all wastes are properly disposed of according to federal, state, and local requirements.
-

- **Solid Waste.** The disposal of all solid waste must comply with Idaho's Solid Waste Management Rules (IDAPA58.01.06). No trash or other solid waste shall be buried, burned, or otherwise disposed of at the project site. These disposal methods are regulated by various state regulations including Idaho's Solid Waste Management Regulations and Standards, Rules and Regulations for Hazardous Waste, and Rules and Regulations for the Prevention of Air Pollution.
- **Water Quality Standards.** Site activities must comply with the Idaho Water Quality Standards (IDAPA 58.01.02) regarding hazardous and deleterious-materials storage, disposal, or accumulation adjacent to or in the immediate vicinity of state waters (IDAPA 58.01.02.800); and the cleanup and reporting of oil-filled electrical equipment (IDAPA 58.01.02.849); hazardous materials (IDAPA 58.01.02.850); and used-oil and petroleum releases (IDAPA 58.01.02.851 and 852).

Petroleum releases must be reported to DEQ in accordance with IDAPA 58.01.02.851.01 and 04. Hazardous material releases to state waters, or to land such that there is likelihood that it will enter state waters, must be reported to DEQ in accordance with IDAPA 58.01.02.850.

- **Ground Water Contamination.** DEQ requests that all activities comply with Idaho's Ground Water Quality Rules (IDAPA 58.01.11), which states that "No person shall cause or allow the release, spilling, leaking, emission, discharge, escape, leaching, or disposal of a contaminant into the environment in a manner that causes a ground water quality standard to be exceeded, injures a beneficial use of ground water, or is not in accordance with a permit, consent order or applicable best management practice, best available method or best practical method."
- **Underground Storage Tanks.** DEQ requests that the installation of all underground storage tanks and piping along with any required testing and owner/operator training comply with Idaho's Rules Regulating Underground Storage Tank Systems (IDAPA 58.01.07)

For questions, contact Gary Stevens, Waste & Remediation Manager, at (208) 769-1422.

#### **6. Additional Notes**

- If an underground storage tank (UST) or an aboveground storage tank (AST) is identified at the site, the site should be evaluated to determine whether the UST is regulated by DEQ. The Panhandle Health District regulates all ASTs over the Rathdrum Prairie aquifer. EPA regulates ASTs at all other areas. UST and AST sites should be assessed to determine whether there is potential soil and ground water contamination. Please call DEQ at 769-1422-, or visit the DEQ website (<http://www.deq.idaho.gov/waste-mgmt-remediation/storage-tanks.aspx>) for assistance.
  - If applicable to this project, DEQ recommends that BMPs be implemented for any of the following conditions: wash water from cleaning vehicles, fertilizers and pesticides, animal
-

facilities, composted waste, and ponds. Please contact DEQ for more information on any of these conditions.

We look forward to working with you in a proactive manner to address potential environmental impacts that may be within our regulatory authority. If you have any questions, please contact me, or any of our technical staff at (208)769-1422.

Dan Redline, Regional Administrator, Coeur d'Alene

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## Northern Lakes Fire Protection District

Proudly serving the areas of and surrounding Hayden and Rathdrum



Hayden Area (208) 772-5711 • Fax: (208) 772-3044  
 Rathdrum Area (208) 687-1815 • Fax (208) 687-2088  
[www.northernlakesfire.com](http://www.northernlakesfire.com)

September 10, 2019

City of Rathdrum Planning & Zoning  
 8047 W. Main Street  
 Rathdrum, ID 83858

Attn: Cary Siess,

The Northern Lakes Fire Protection District has reviewed the preliminary subdivision request Westwood Pines, 112 single-family residential lots, located at Meyer Rd and Highway 53 in Rathdrum.

*The Fire District approves of the preliminary subdivision with the following requirements.*

- **Final subdivision drawings shall be submitted to Northern Lakes Fire prior to start of construction.**
- Roadways shall be reviewed by Fire District prior to start of construction. Roads with fire hydrants shall be no less than twenty-six (26) feet wide.
- Fire flows shall meet the requirements of the 2015 Idaho Fire Code. Dwellings smaller than 3,600 sq. ft. require a fire flow of 1,000 gpm for a duration of 1 hour. Dwellings larger than 3,600 sq. ft. require higher fire flows depending on the square footage. Refer to the 2015 Idaho Fire Code Appendix B, Section B105 and table B105.1.
- Proposed locations of fire hydrants shall be shown on final site plan, these locations shall be approved by Fire District prior to start of construction. Fire hydrants shall be spaced with a separation of no more than 500 feet.
  - **There shall be fire hydrants installed at entrances to subdivision and also internally within the subdivision.**
  - **Fire hydrants shall be operational before the start of construction to any dwellings.**
  - **Fire hydrants shall be outfitted with 5" Storz adapter with cover and hydrant marking flags.**
- Address numbers shall be provided and visible from access road fronting the property.
- **Prior to final subdivision approval and start of construction the subdivision approval fee of \$47.00/hr. minimum of one hour and \$30.00 per dwelling unit shall be paid to the Fire District.**
- If any type of construction occurs other than 1 and 2 family dwellings homes will be reviewed for the need for fire sprinkler systems, this may require larger water line sizing.
- 2015 Idaho Fire Code requirements shall apply when building permits are applied for. Additional requirements may apply depending on size and type of occupancy constructed.

Thank you,

*Tyler Drechsel*

If you have any questions, please call 208-772-5711.

125 West Hayden Avenue • Hayden Lake, Idaho 83835

Tyler Drechsel  
 Fire Marshal

**From:** [David Callahan](#)  
**To:** [Allyson Andrade](#)  
**Cc:** [Ray Kimball](#); [Save](#); [mari@rathdrum.org](mailto:mari@rathdrum.org)  
**Subject:** RE: Notice of Application  
**Date:** Thursday, August 29, 2019 3:11:58 PM  
**Attachments:** [image001.gif](#)

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We have no comments or concerns.

**David Callahan, AICP**  
Director

**Kootenai County**  
Community Development

451 N Government Way • P.O. Box 9000  
Coeur d'Alene, Idaho 83816-9000

Phone: 208 446 1082 • Email: [dcallahan@kcgov.us](mailto:dcallahan@kcgov.us)  
Mobile: 208 660 3029

**From:** Allyson Andrade <[aandrade@whipplece.com](mailto:aandrade@whipplece.com)>  
**Sent:** Tuesday, August 27, 2019 3:27 PM  
**Cc:** Ray Kimball <[rkimball@whipplece.com](mailto:rkimball@whipplece.com)>; Save <[save@whipplece.com](mailto:save@whipplece.com)>; [mari@rathdrum.org](mailto:mari@rathdrum.org)  
**Subject:** Notice of Application

Dear agency representative,

As required by the City of Rathdrum's subdivision process, on behalf of Viking Construction, we are soliciting comments for the attached proposed 110 lot single family residential subdivision application. Please note that this subdivision will also result in two remainder lots which have an existing zoning of C-2 and R-3 respectively.

Please reply by September 10th to [info@whipplece.com](mailto:info@whipplece.com) with any comments.

Best Regards,

Ray Kimball, P.E.  
Whipple Consulting Engineers

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5

**From:** [Allyson Andrade](#)  
**To:** [Ray Kimball](#)  
**Cc:** [Save](#)  
**Subject:** FW: Notice of Application  
**Date:** Tuesday, September 3, 2019 9:40:12 AM  
**Attachments:** [image002.gif](#)  
[image003.gif](#)  
[image005.jpg](#)  
[image006.png](#)

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Ray,

Please see the below.

Thanks,

Allyson Andrade  
Marketing Communications Coordinator  
[Whipple Consulting Engineers, Inc.](#)  
Phone: 509.893.2617 | Fax: 509.926.0227

---

**From:** Craner, Angela - Rathdrum, ID <Angela.Craner@usps.gov>  
**Sent:** Tuesday, September 03, 2019 8:43 AM  
**To:** Allyson Andrade <aandrade@whipplece.com>  
**Subject:** RE: Notice of Application

Thank you –

Please let me know when we could get together to go over where the CBU units will be placed.  
Thank you

*Angela Craner  
Postmaster (Acting)  
Rathdrum Post Office  
13867 W. Highway 53  
Rathdrum, Idaho 83858*

---

*(208) 687-1281*

*1260 Coordinator MPOO Area 6*

*Postmaster  
Valleyford Post Office  
12809 S. Madison Rd  
Valleyford, Wa 99036  
(509) 922-3881*

---

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**From:** Allyson Andrade [<mailto:aandrade@whipplece.com>]  
**Sent:** Tuesday, August 27, 2019 3:27 PM  
**Cc:** Ray Kimball <[rkimball@whipplece.com](mailto:rkimball@whipplece.com)>; Save <[save@whipplece.com](mailto:save@whipplece.com)>; [mari@rathdrum.org](mailto:mari@rathdrum.org)  
**Subject:** [EXTERNAL] Notice of Application

Dear agency representative,

As required by the City of Rathdrum's subdivision process, on behalf of Viking Construction, we are soliciting comments for the attached proposed 110 lot single family residential subdivision application. Please note that this subdivision will also result in two remainder lots which have an existing zoning of C-2 and R-3 respectively.

Please reply by September 10th to [info@whipplece.com](mailto:info@whipplece.com) with any comments.

Best Regards,

Ray Kimball, P.E.  
Whipple Consulting Engineers

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**PUBLIC WORKS DEPARTMENT**

8047 W. Main Street  
Rathdrum, ID 83858  
P 208.687.2700  
F 208.687.1377

# Memo

**To:** File  
**From:** Kevin Jump  
**CC:**  
**Date:** December 10, 2019  
**Re:** Westwood Pines – force main alignment

---

As part of the concept planning for the Westwood Pines residential development (NE corner of SH53 & Meyer Road), there was discussion about the construction of a temporary sanitary sewer lift station (on-site) with a sewer force main that would be extended under SH53 and connect to the City's sewer collection system on Commercial Park Drive.

As the project is advancing to engineering design, the developer is seeking to revise the sewer force main alignment so that it runs through the City's Pine Grove Cemetery property and connect at the Timberline Street cul-de-sac.

## **Pine Grove Cemetery - Agreement**

The Pine Grove Cemetery is owned and operated by the City of Rathdrum. 9.47 acres of the Pine Grove Cemetery property is subject to the terms of an agreement with Gordon Sylte. From a review of the agreement, there are financial terms for the sale of burial sites as well as plat restrictions which both cover burial sites/plots and the use of the property exclusively as a public cemetery.

There are vehicular access aisles within the platted Pine Grove Cemetery.

## **Sewer Force Main Alignment**

It is Rathdrum City Staff's recommendation that the sanitary sewer force main can be extended through the Pine Grove Cemetery with the following conditions:

- The sewer force main alignment is required to be contained within the noted/platted vehicle access aisles.
- An all-weather access roadway is required along the west property line of the Pine Grove Cemetery – pertains only to the sewer force main alignment.
- A 16-foot wide asphalt access aisle is required for the east-to-west sewer force main alignment. The asphalt aisle shall be 2" of asphalt pavement over 4" of  $\frac{3}{4}$ " crushed stone. The asphalt pavement, crushed stone subbase and underlying subgrade is required to be prepared in accordance with Rathdrum's asphalt pavement construction requirements for public roadways.

**PUBLIC WORKS DEPARTMENT**

8047 W. Main Street  
Rathdrum, ID 83858  
P 208.687.2700  
F 208.687.1377

# Memo

**To:** File  
**From:** Kevin Jump  
**CC:**  
**Date:** February 12, 2020  
**Re:** Westwood Pines – Traffic LOS Analysis for SH53-Meyer Intersection

---

Whipple Consulting Engineers submitted a Traffic Level of Service (LOS) Analysis, specifically as it pertains to the intersection of SH53 & Meyer Road.

This memo includes a summary of project information along with a review of the submitted Traffic LOS Analysis.

### **Traffic Circulation Impact Fees**

The City of Rathdrum collects traffic impact fees for new construction building permits (including remodel permits). The collected traffic impact fees are uniformly applied regardless of a project location.

As an example, for each single-family residential building permit, we collect \$ 1,344.00 exclusively for traffic impact fees. These collected traffic impact fees can be used on any capital improvement project identified in the impact fee study – to the financial limits therein stated.

On our Traffic Impact Fee Study, the intersection of SH53-Meyer is identified as a capital improvement project. The City of Rathdrum has adopted “D” as its minimal allowable LOS for major roadways/intersections.

### **Proposed Improvements to the SH53-Meyer Road Intersection**

Rathdrum Public Works is seeking to initiate a capital improvement project for the SH53-Meyer Road intersection. There are several coordination steps which need to occur for this project to successively be advanced.

From a scoping standpoint, the proposed improvements to the intersection include, but are not necessarily limited to: fully-actuated traffic signal, added left-turn lane bays for each approach, shoulder improvements and pedestrian pathway extension.

From a scheduling standpoint, we anticipate the following:

- Approval of Professional Services Agreement (engineering): February '20
- Start Topographical Survey & Preliminary Engineering: March '20
- Traffic Warrant Analysis – submitted to ITD: May '20
- Preliminary Design (complete): October '20
- Final Design (complete): January '21
- Start Construction: March '21
- Construction Completion: June '22

The project timeline is impacted by several factors:

- Coordination with & approval from the Idaho Transportation Department (ITD) to advance the project
- timely acquisition of public right-of-way
- timely relocation of impacted utilities
- unforeseen subsurface impacts – geotechnical and/or environmental
- Project Funding

#### **Review of Traffic LOS Analysis – SH53 & Meyer Road Intersection**

The **existing traffic conditions** for the SH53-Meyer Road intersection are reported as:

Northbound (NB) Meyer Road:	D (delay of 30 seconds ± for both AM Peak and PM Peak)
Southbound (SB) Meyer Road:	A (AM Peak) and B (PM Peak)
Westbound (WB) SH53:	A (free-flowing in WB direction)
Eastbound (EB) SH53:	A (free-flowing in EB direction)

From a recent Viking development project, Timber Landings, it is noted that Viking is building, on average, 32 single-family homes per year. Thus, Whipple's application of 30 developed homesites per year accurately represents the current market conditions. From a traffic analysis standpoint, if Westwood Pines existed today with a **50% population** (60 residential homesites occupied), then the following traffic conditions would exist:

Northbound (NB) Meyer Road:	D/E (delay of 30-to-45 seconds ± for both AM Peak and PM Peak)
Southbound (SB) Meyer Road:	D (AM Peak) and C (PM Peak)
Westbound (WB) SH53:	A (free-flowing in WB direction)
Eastbound (EB) SH53:	A (free-flowing in EB direction)

From a traffic analysis standpoint, if Westwood Pines existed today with a **100% population** (110 residential homesites occupied), then the following traffic conditions would exist:

Northbound (NB) Meyer Road:	F (delay of 55-to-60 seconds ± for both AM Peak and PM Peak)
Southbound (SB) Meyer Road:	D (AM Peak) and C (PM Peak)
Westbound (WB) SH53:	A (free-flowing in WB direction)
Eastbound (EB) SH53:	A (free-flowing in EB direction)

### **Westwood Pines Development**

From our experience, we anticipate the following schedule for the development of Westwood Pines:

- |   |              |
|---|--------------|
| - Interior Public Infrastructure Construction (completed by): | November '20 |
| - Start New Home Construction:                                | November '20 |
| - Completion of First Home Construction:                      | June '21     |

Applying current market conditions, it is estimated that approximately 3 new homes per month will be constructed (32 new homes per year = 2.7 homes per month).

### **Recommendation**

Optimistically, we anticipate completing the SH53-Meyer intersection improvements by June '22. Applying the above conditions, the Westwood Pines subdivision will, likewise from an economically optimistic standpoint, will be 30% developed (100% occupancy of 30% of the homesites) When the SH53-Meyer Road intersection improvements are complete and functional.

Any traffic added to an intersection will degrade its LOS. While adhering to the City of Rathdrum's minimal LOS of "D", we believe that the Westwood Pines subdivision could obtain a 50% occupancy level before we risk the intersection being out of tolerance with our adopted LOS "D" standard.

Therefore, it's my recommendation that the following conditions be applied:

- Westwood Pines be limited to the issuance of 60 single-family residential building permits until capital improvements are completed for the SH53-Meyer intersection.
- Any non-residential development, within Westwood Pines, will require a new, updated traffic LOS analysis.
- The Westwood Pines development obtain ITD approval for its submitted traffic impact analysis for both SH53-Meyer Road intersection and the new, proposed Cloverdale Street access point onto SH53.
- To best accommodate the proposed SH53-Meyer Road intersection improvement project, Westwood Pines shall donate (at no cost to the City of Rathdrum and/or ITD), all necessary public right-of-way and easements on the north-side of SH53 and east-side of Meyer Road.





April 18, 2019

W.O. No. 2019-2290

City of Rathdrum  
8047 Main Street Unit 1  
Rathdrum, ID 83858

Attn: Kevin Jump, P.E.

Re: **Westwood Village Subdivision**  
**Highway 53 & Meyer Road, Rathdrum**  
**Trip Generation & Distribution Letter**

Dear Mr. Jump,

This Trip Generation and Distribution Letter (TGDL) is for the proposed Westwood Village Subdivision. This letter will establish the anticipated trip generation and distribution for the development as shown on Figure 2, Preliminary Site Plan. This report will follow the standards for traffic letters as required by the Institute of Transportation Engineers, City of Rathdrum and the Idaho Transportation Department.

### **PROJECT DESCRIPTION**

The project proposes to develop 50.00 ac +/- into a 110-lot single family residential development. The property is located on the east side of Meyer Road approximately 700 feet north of the intersection of Highway 53 and Meyer Road. The property is currently undeveloped with trees, field grass and weeds. The project proposes to access Meyer Road with a proposed east/west street. The project proposes to access Highway 53 with a proposed north/south street located 1,600 feet east of the intersection of Highway 53 and Meyer Road. The project does not have frontage on Highway 53. Within the development there are two proposed loop roads that connect to the proposed east/west street that connects to Meyer Road. The proposed east/west road is proposed to extend east from Meyer Road to the east boundary of the project for a possible future connection. Please see Figure 2, Preliminary Site Plan.

### **VICINITY / SITE PLAN**

The site is currently listed in the City of Rathdrum Comprehensive Plan and zoned as R-2S, R-3 and C-2. The subject properties are located on a portion of the SW ¼ of Section 29, T. 52 N., R. 04 W., B.M., within the City of Rathdrum, Idaho. The parcel numbers for the subject properties are R-0000-029-6200 and R-0000-029-6500. The surrounding areas to the west and southwest are a mix of residential, public and commercial land uses. The areas to the north, south and east are mostly rural land uses.

## **TRIP GENERATION AND DISTRIBUTION**

### **Trip Types**

The proposed use is a single family residential development; ITE has developed data regarding various trip types that all developments experience. These are found in several places, however, for this analysis the *Trip Generation Manual 10<sup>th</sup> Edition* as well as the *Trip Generation Handbook* were used to develop the criteria for this analysis.

Generally, all existing and proposed developments will be made up of one or more of the following four trip types: new (destination) trips, pass-by trips, diverted trips, and shared (internal trips). In order to better understand the trip types available for land access a description of each specific trip type follows.

**New (Destination) Trips** - These types of trips occur only to access a specific land use such as a new retail development or a new residential subdivision. These types of trips will travel to and from the new site and a single other destination such as home or work. This is the only trip type that will result in a net increase in the total amount of traffic within the study area. The reason primarily is that these trips represent planned trips to a specific destination that never took trips to that part of the City prior to the development being constructed and occupied. This project will develop new trips.

**Pass-by Trips** - These trips represent vehicles which currently use adjacent roadways providing primary access to new land uses or projects and are trips of convenience. These trips, however, have an ultimate destination other than the project in question. They should be viewed as customers who stop in on their way home from work. An example would be on payday, where an individual generally drives by their bank every day without stopping, except on payday. On that day, this driver would drive into the bank, perform the prerequisite banking and then continue on home. In this example, the trip started from work with a destination of home, however on the way, the driver stopped at the grocery store/latte stand and/or bank directly adjacent to their path. Pass-by trips are most always associated with commercial/retail types of development along major roadways. Therefore, for this project pass-by trips will not be considered.

**Diverted (Linked) Trips** - These trips occur when a vehicle takes a different route than normal to access a specific facility. Diverted trips are similar to pass-by trips, but diverted trips occur from roadways which do not provide direct access to the site. Instead, one or more streets must be utilized to get to and from the site. For this project, no diverted trips are anticipated.

**Shared Trips** - These are trips which occur on the site where a vehicle/consumer will stop at more than one place on the site. For example, someone destined for a certain shop at a commercial site may stop at a bank just before or after they visit the shop that they went to the site to visit. This trip type reduces the number of new trips generated on the public road system and is most commonly used for commercial developments. Since the project has only one land use and no cross-access driveways with other land uses, no shared trips were considered.

---

**Trip Generation Characteristics for the Proposed Project**

As noted earlier, trip generation rates for the AM and PM peak hours are determined by the use of the *Trip Generation Manual, 10<sup>th</sup> Edition* published by the Institute of Transportation Engineers (ITE). The purpose of the *Trip Generation Manual* is to compile and quantify empirical data into trip generation rates for specific land uses within the US, UK and Canada.

For the proposed 110-unit single family residential development, Land Use Code (LUC) #210 Single Family Detached Housing was used to establish the number of potential trips generated by the apartment units. The trip generation rates and the anticipated number of AM & PM peak hour trips for the proposed project are shown on Table 1.

**Table 1-Trip Generation Rates for LUC # 210 – Single Family Detached Housing**

Dwelling Units	AM Peak Hour Trips			PM Peak Hour Trips		
	Vol. @ 0.74/ Unit	Directional Distribution		Vol. @ 0.99/ Unit	Directional Distribution	
		25% In	75% Out		63% In	37% Out
110	82	21	61	109	69	40
<b>Average Daily Trip Ends (ADT)</b>						
<b>Dwelling Units</b>	<b>Rate</b>	<b>ADT</b>				
110	9.44	1,039				

As shown in Table 1, the proposed development is anticipated to generate 82 trips in the AM peak hour with 21 trips entering the site and 61 trips exiting the site. In the PM peak hour, the proposed project is anticipated to generate 109 trips with 69 trips entering the site and 40 trips exiting the site. The proposed project is anticipated to generate 1,039 average daily trips to/from the project.

**TRIP DISTRIBUTION**

As shown on the site plan, the site will be accessed by Meyer Road and Highway 53 (please see Figure 2 Site Plan). It is anticipated that the residents of the site will generally use the following roadways:

**Meyer Road** is generally a north/south, two-way, 2-lane collector that extends north from 16<sup>th</sup> Avenue through Poleline Avenue, Prairie Avenue, Hayden Avenue, Wyoming Avenue, Lancaster Road, and Boekel Road before terminating just after Highway 53. Meyer Road serves commercial, residential and rural land uses. The posted speed limit on Meyer Road in the project area is 35 MPH.

**Highway 53** is generally an east/west, two-way, 2-lane highway. Trent Avenue in Washington turns into Highway 53 when it reaches the Idaho border. Highway 53 then extends east through Prairie Avenue, Pleasant View Road, McGuire Road and Greensferry Road before jetting south

Westwood Village  
Trip Generation & Distribution Letter  
April 18, 2019  
Page 4 of 4

in Rathdrum and continuing east through Meyer Road to U.S. Highway 95. Highway 53 primarily serves residential, commercial and rural land uses. The posted speed limit on Highway 53 in the project area is 35 MPH west of Meyer Road and 55 MPH east of Meyer Road.

Considering many factors such as the surrounding transportation facilities, typical commuting patterns, and existing development in the area, traffic for the proposed development is anticipated as follows: 50% of the trips are anticipated to go to/from the east via Highway 53, 20% of the trips are anticipated to go to/from the south via Meyer Road and 30% of trips are anticipated to go to/from the west via Highway 53.

### CONCLUSIONS AND RECOMMENDATIONS

It is anticipated that the proposed project will generate 82 trips in the AM peak hour and 109 trips in the PM peak hour trips. Based upon the number of anticipated trips, and the distribution of those trips on a state highway, we believe that the proposed project will have a minimal impact on the transportation system. Therefore, we recommend that the project complete frontage improvements on Meyer Road and be allowed to move forward without further traffic analysis.

Should you have any questions related to this document please do not hesitate to call at (509) 893-2617.

Sincerely,  
WHIPPLE CONSULTING ENGINEERS, INC.

A circular professional engineer seal for Todd R. Whipple, State of Idaho, No. 8692. The seal is stamped in blue ink and includes the text "PROFESSIONAL ENGINEER", "REGISTERED", "STATE OF IDAHO", and "TODD R. WHIPPLE". A handwritten signature in blue ink is written over the seal, and the date "4/18/19" is written to the right of the seal.

Todd R Whipple, P.E.

TRW/tae

encl. Appendix (Vicinity Map, Preliminary Site Plan, Trip Dist %)

cc: Sponsor  
File

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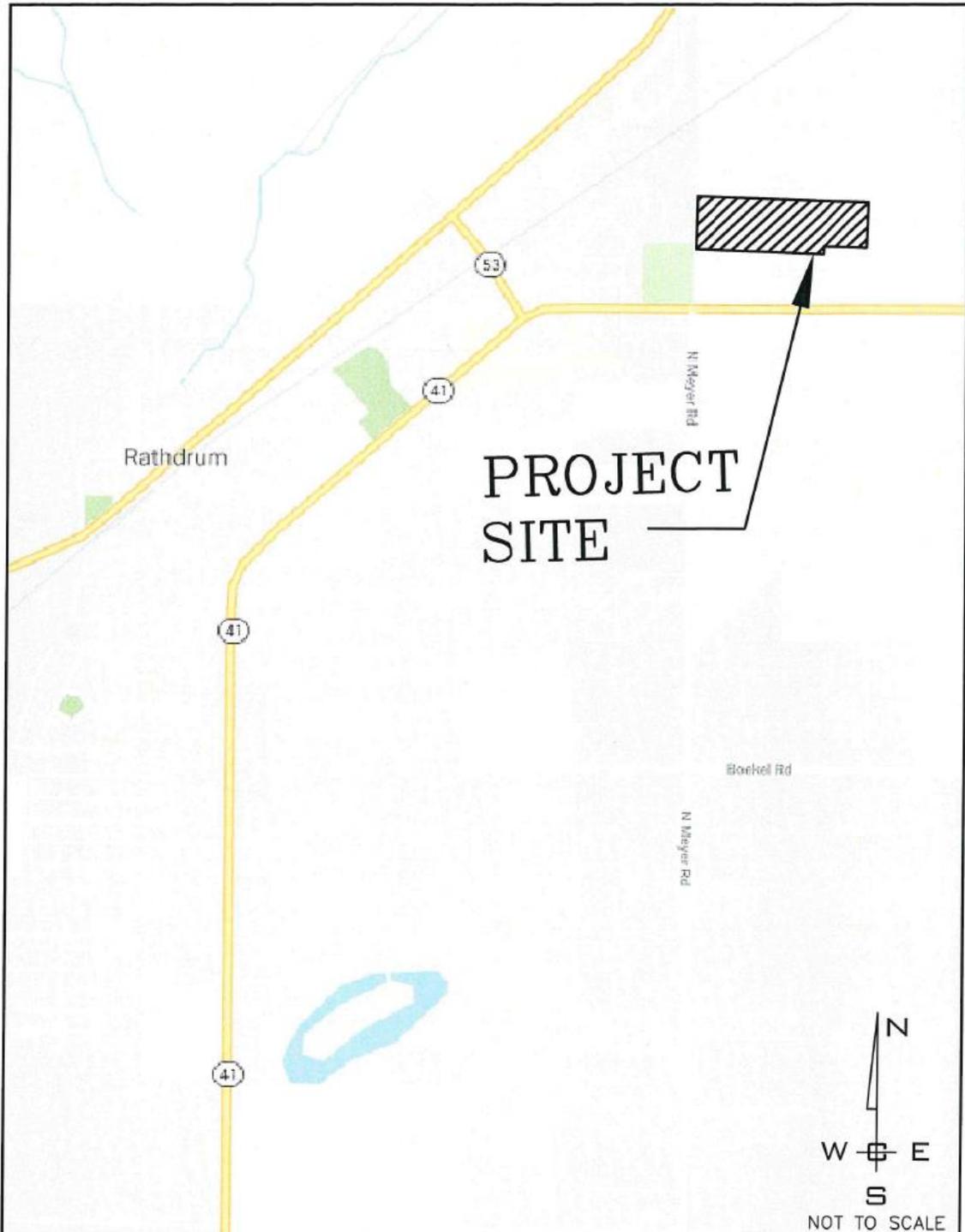
# **APPENDIX**

1. Vicinity Map

2. Site Plan

3. Trip Distribution by Percentage





PROJ #: 19-2290  
 DATE: 04/02/19  
 DRAWN: TAE  
 APPROVED: TRW

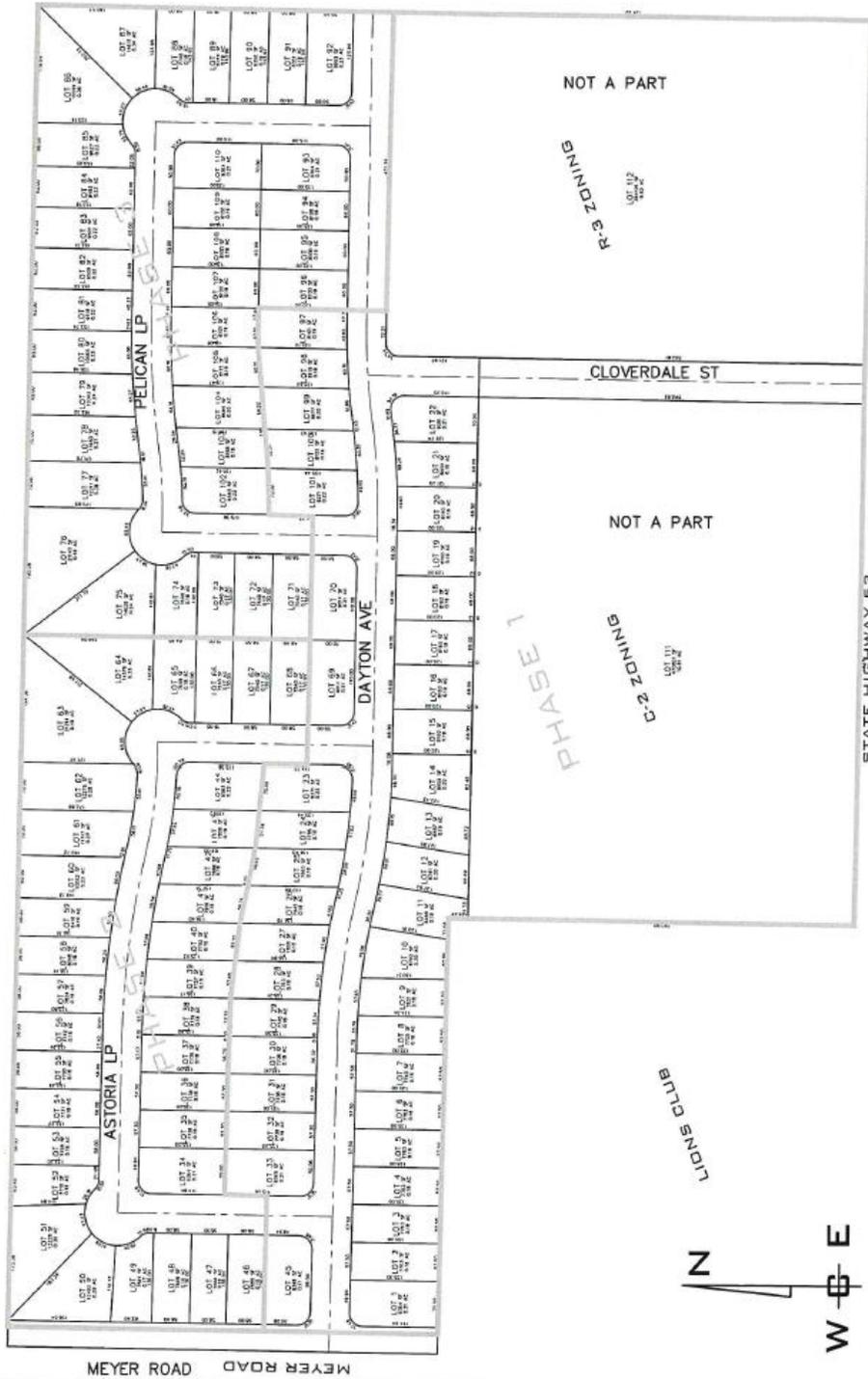
**TRIP GENERATION & DISTRIBUTION**  
**WESTWOOD PINES**  
**MEYER ROAD & HIGHWAY 53**  
**RATHDRUM, IDAHO**

**WCE**  
 WHIPPLE CONSULTING ENGINEERS  
 CIVIL AND TRANSPORTATION ENGINEERING  
 21 SOUTH PINES ROAD  
 SPOKANE VALLEY, WASHINGTON 99206  
 PH: 509-693-2617 FAX: 509-926-0227

FIGURE 1

VICINITY MAP

SW 1/4 SEC. 29, T.52N., R.04 W., B.M.  
**WESTWOOD PINES**  
**PRELIMINARY SUBDIVISION PLAN**  
 LOCATED IN THE UNINCORPORATED  
 SEC 29, T52N, R04W  
 CITY OF RATHDRUM, IDAHO



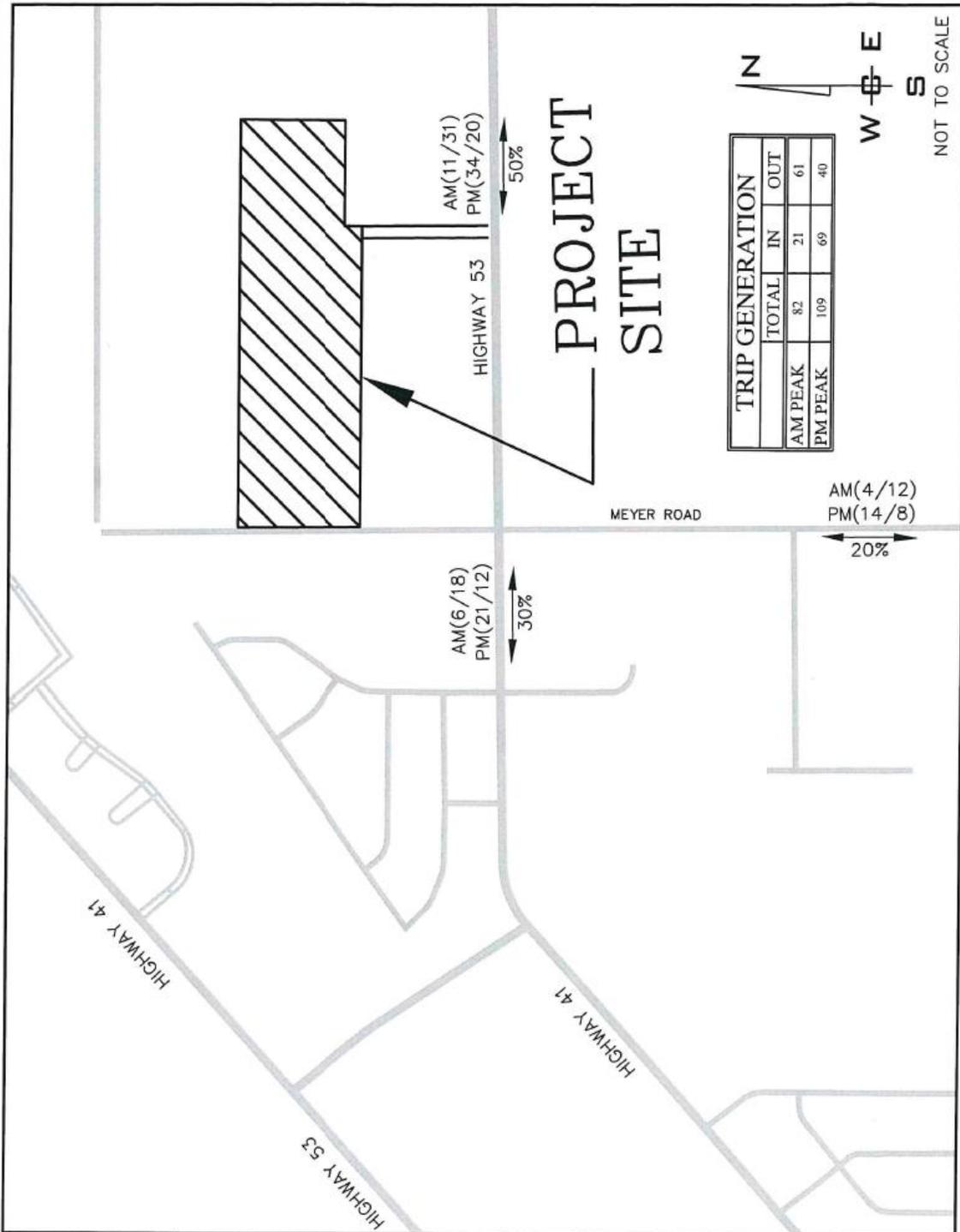
PROJ #: 19-2290  
 DATE: 04/02/19  
 DRAWN: TAE  
 APPROVED: TRW

**TRIP GENERATION & DISTRIBUTION**  
**WESTWOOD PINES**  
**MEYER ROAD & HIGHWAY 53**  
**RATHDRUM, IDAHO**

**WCE**  
 WHIPPLE CONSULTING ENGINEERS  
 CIVIL AND TRANSPORTATION ENGINEERING  
 21 SOUTH PINES ROAD  
 SPOKANE VALLEY, WASHINGTON 99206  
 PH: 509-893-2617 FAX: 509-926-6227

FIGURE 2

PRELIMINARY SITE PLAN



PROJ #: 19-2290  
 DATE: 04/02/19  
 DRAWN: TAE  
 APPROVED: TRW

**TRIP GENERATION & DISTRIBUTION**  
**WESTWOOD PINES**  
**MEYER ROAD & HIGHWAY 53**  
**RATHDRUM, IDAHO**

**FIGURE 3**      **PROJECT TRIP DISTRIBUTION**

**WCE**  
 WHIPPLE CONSULTING ENGINEERS  
 CIVIL AND TRANSPORTATION ENGINEERING  
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 SPOKANE VALLEY, WASHINGTON 99206  
 PH: 509-893-2617 FAX: 509-926-0227



## AQUILA GEOSCIENCES, INC.

P.O. Box 544  
Potlatch, Idaho 83855  
(208) 875-1175

September 7, 1999

Mr. Bob McComb  
4560 McGuire Rd.  
Post Falls, Idaho 83854

**SUBJECT:** Transmittal of Report of on Ground Penetrating Radar Survey at the Rathdrum Lion's Club Property at Rathdrum, Idaho.

Dear Bob:

Here is the report on the GPR survey we did for you at Rathdrum, Idaho, on August 23. I've enclosed one copy of the report. If you need more with colored prints of the radar records, you would be better off getting copies of the enclosed report than having us print another one.

We will maintain a copy of the radar records in digital form for at least two years. Actually we'll probably keep them until we retire, since we never seem to throw old job stuff away.

I've stamped the report, figuring it may be beneficial for you if I do. Please do not hesitate to call, write, or email me if you have questions or need more information.

Margaret and I hope this is adequate for your needs.

Sincerely,

Kent R. Johnson, Ph.D., P.G.  
Geologist/Geophysicist

---



## AQUILA GEOSCIENCES, INC.

P.O. Box 544  
Potlatch, Idaho 83855  
(208) 875-1175

September 3, 1999

Rathdrum Lions Club  
c/o Bob McComb  
4560 McGuire Rd.  
Post Falls, Idaho 83854

### **Record of a Ground Penetrating Radar (GPR) Survey conducted at the Rathdrum Lions' Club site in or adjacent to the City of Rathdrum, Idaho**

Aquila Geosciences acquired and interpreted Ground Penetrating Radar (GPR) records at a site owned by the Rathdrum Lions' Club that either is in or is adjacent to the City of Rathdrum on August 23, 1999. The purpose of the survey was to attempt to locate unmarked graves attributed to be those of Chinese railway workers from the late 1800's.

#### **Data Acquisition**

The GPR data were acquired using a Geophysical Survey Systems, Inc. (GSSI) SIR System-2000 GPR unit equipped with a 400 MHZ. antenna. The data were stored in "RADAN<sup>®</sup>" format on the GPR unit's internal hard drive. The data were downloaded to a microcomputer for printing and archived to a Zip<sup>™</sup> Drive for storage.

The first set of lines (files 42-45) were acquired in the existing cemetery across the gravel road from the Lions Club property. These were used for calibrating the main survey. The rest of the radar lines (files 46 and 50-63) were acquired at the main survey site on the Lions Club property. There are no files 47, 48, and 49.

Acquisition parameters that were common for all lines were:

Samples/Scan 512

Bits/Sample 8

Scans/Second 32

Range: 100 nsec

Vert IIR LP Filter 1000 MHZ (Vertical Infinite Impulse Response Low Pass Filter)

Vert IIR HP Filter 30 MHZ (Vertical Infinite Impulse Response High Pass Filter)  
Horz IIR Stack 2 (Horizontal Smoothing Infinite Impulse Response Stack)  
Gain Points: 5

There were five gain points per scan for all records, however the gains varied between the cemetery and the Lions Club property as follows:

- Files 42-45 all had the same gain settings.
- Files 46-63 all had the same gain settings.

We also adjusted the Range of the recording in an attempt to determine what might be the optimum Range for detecting anomalies resulting from graves.

- File 42 was recorded with a Range of 100 nanoseconds.
- Files 43 and 44 were recorded with a Range of 60 nanoseconds.
- Files 45-63 were recorded with a Range of 70 nanoseconds.

(Note again that there are no files 47, 48, and 49.)

Files 42 and 43 were acquired over a grave with a marker dating it as 1886. We have not included a print of File 42 because, other than indicating that the recording Range was too great, it is not particularly informative. On the print of File 43 the grave is marked by the second of the three vertical white dashed lines at the top of the record. The entire area of this line had been disturbed, possibly by other, unmarked, graves. However, the dark colors between two thirds and three quarters of the way down to the bottom of the record probably represent the remains.

Files 44 and 45 were recorded over another grave with no date on the marker. This was thought to be a newer grave. Both graves are marked by a disturbance of the natural layering. This disturbance is not just a disruption of the reflections, such as seen on either side of the grave (which may be partially due to minor surface topography), but also a reduction in amplitude.

Files 46, and 50-63 were acquired at the area in question at the Lions Club property. Traverse lines were laid out oriented approximately North-South, based on the assumptions that graves would be oriented East-West. Figure 1 is a sketch map that shows the locations of the lines. Each line is approximately 116 feet long, and there are 5 feet between the lines. The first page of the print of each line is the file header, which gives the acquisition parameters. The next two pages are the actual print of the radar record. North (approximately) is to the left in all of the prints of the radar records. The short dashed lines at intervals across the tops of the radar records are ten (10) feet apart. Note that the radar unit scans the ground

at a constant rate of 32 scans per second. The number of scans collected depends on how fast the person pulling the antenna walks, and it is difficult to walk at an absolutely constant rate. Each record begins and ends with a series of horizontal, parallel reflections. These represent periods of time during which the antenna was stationary. The vertical scale is as indicated in the file header. Due to a problem with the software we could not print out a vertical scale. Please remember that the vertical scale for all of the prints is in two-way-travel time, not in depth. This is because we did not have any way to accurately calibrate the velocity of radar waves in the subsurface at this site. That is necessary to make valid depth calculations from the radar data.

### Interpretation

Anomalies are marked by a red arrow on the prints of the radar records and these anomalies are also located on the map in Figure 1. Several of the anomalies can be traced to shallow objects, mostly tree roots or stumps (R/S in Figure 1). The interpretation of most of the anomalies is subjective, based on examination of the radar records and comparison with those recorded in the cemetery. Anomalies labeled ??? were thought unlikely, or least likely, to be burials. Those labeled ●? were interpreted to probably not be burials. Those labeled POS? (in a box) were considered to be possible burial sites, based on a similarity with the record over known graves in the cemetery.

The interpretation of the records in terms of burial sites is complicated by the observation that burials made at this site were probably not in coffins, but the remains were simply wrapped in cloth. We expect this to produce a different radar signature than a burial in a coffin, which is what we expect the ones in the cemetery to be.

Another consideration in the interpretation is that grave diggers might be more inclined to dig where there were fewer tree roots. This *may* have been the case in the southern part of the site, where most of the "possible burial" anomalies are located. We had the impression there had been fewer trees in this area. This could make the two "possibles" in the northwest part of the site seem less likely. However, we caution that the anomaly patterns are not really diagnostic enough to reach a firm conclusion.

### Summary

The GPR survey at the Rathdrum Lions Club property in or adjacent to the City of Rathdrum, Idaho, was successful. Data of reasonable quality were acquired. Some

of the radar anomalies seen on the records could represent unmarked graves.

The results of this survey are intended for use in project planning and management. The geophysical surveys we do are intended to guide direct sampling, not replace it. Surveys are designed based on information known about the survey area prior to conducting the survey. All interpretations are opinions based on inferences from electrical measurements and the accuracy or correctness of such interpretations cannot be guaranteed.

### Acknowledgments

We would like to express our sincerest thanks to Bob Laverdure, Norm Hiesturman, Cheryl Hiesturman, Bob McComb, and John Hieturman for their willing and able assistance during the survey. They made our job much easier and the work much more enjoyable.

Please feel free to call should you have any questions.

Sincerely,



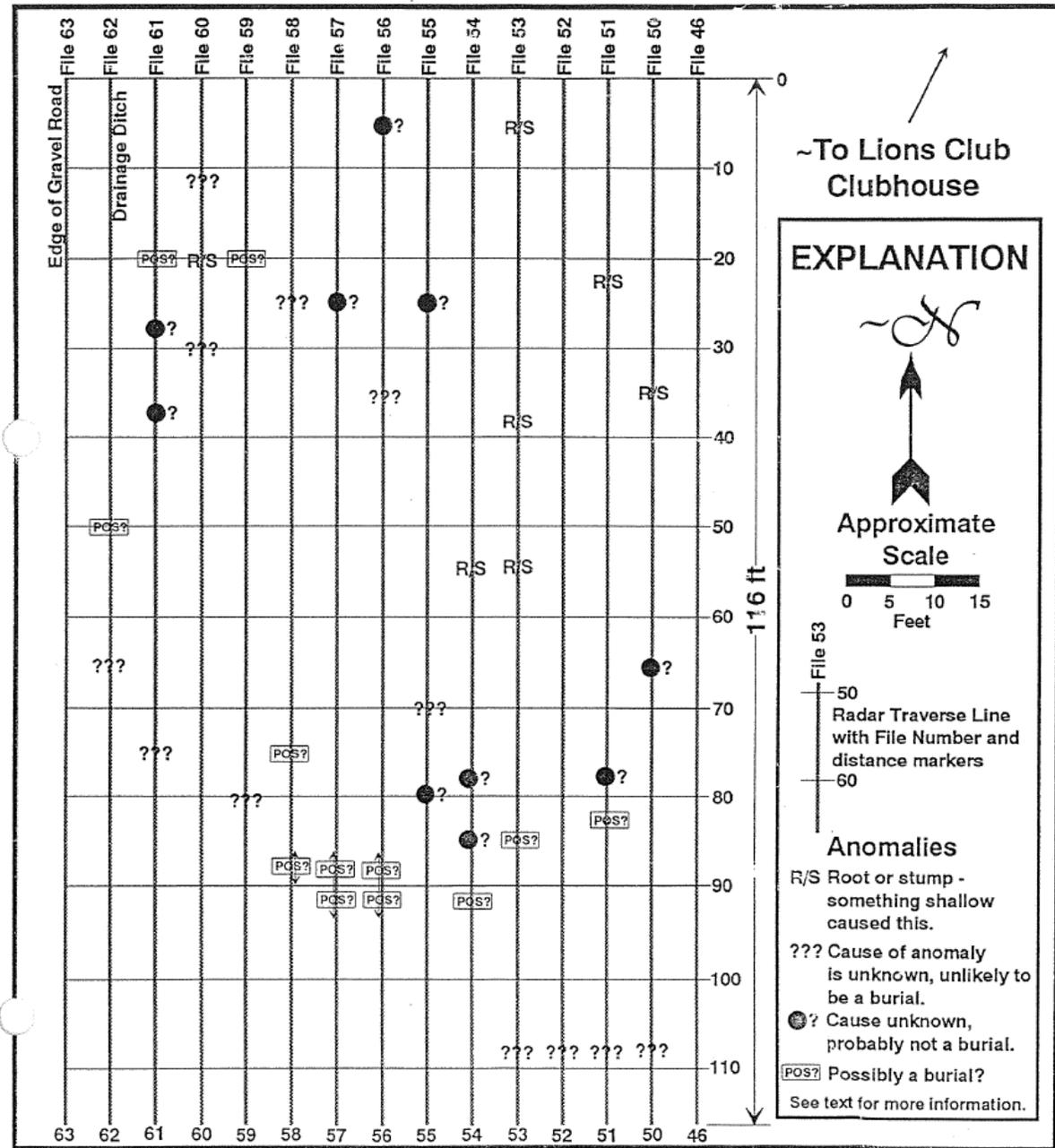
Kent R. Johnson, Ph.D., P.G.  
Geologist/Geophysicist



Rathdrum Lions Club  
 Ground Penetrating Radar Survey  
 Rathdrum, Idaho  
 August 23, 1999

Figure 1

Base Map



# WCE

Whipple Consulting Engineers, Inc

21 S. Pines Road  
Spokane Valley, WA 99206  
Ph 509-893-2617 Fax 509-926-0227



## MEMORANDUM

<b>TO:</b>	Kevin Jump, P.E., City of Rathdrum		
<b>FROM:</b>	Todd R. Whipple, P.E. Ben Goodmansen, E.I.T., Ken Kim, E.I.T.		
<b>DATE:</b>	February 7, 2020		
<b>PROJECT NO:</b>	19-2290	<b>NAME:</b>	Westwood Pines Subdivision
<b>REGARDING:</b>	LOS Analysis with/without the Project		

This memo is intended to provide quick level of service (LOS) calculations at the intersection of Highway 53 & Meyer Road, in order to provide some insight as to the impact that the project may have on the intersection without the anticipated signal

The Levels of Service at the intersection was calculated using the methods from the *Highway Capacity Manual 6<sup>th</sup> Edition*, as implemented in Synchro, version 9 - Build 915.

*Table 1 - Year 2020 Levels of Service, without the Project*

INTERSECTION	(S)ignalized (U)nsignalized	AM Peak Hour		PM Peak Hour	
		Delay (sec)	LOS	Delay (sec)	LOS
Highway 53 & Meyer Road	U	32.1	D	29.6	D

The project is a 110-lot subdivision that is anticipated to build 30 lots +/- per year, and take approximately 3 to 4 years to complete based upon market conditions. For this analysis we applied all of the project trips, and about half of the project trips (60 lots) as if they were all built today, and the ITE trip generation was achieved.

*Table 2 - Year 2020 Levels of Service, with half the Project (60 lots)*

INTERSECTION	(S)ignalized (U)nsignalized	AM Peak Hour		PM Peak Hour	
		Delay (sec)	LOS	Delay (sec)	LOS
Highway 53 & Meyer Road	U	43.5	E	29.9	D

As shown in Table 2 with about half of the project (60 lots) at the ITE trip generation rate the intersection is anticipated to operate at an acceptable level of service. This equates to about a 2-year period of operation that the signal can be installed

*Table 3 – Year 2020 Levels of Service, with the Full Project (110 lots)*

INTERSECTION	(S)ignalized (U)nsignalized	AM Peak Hour		PM Peak Hour	
		Delay (sec)	LOS	Delay (sec)	LOS
Highway 53 & Meyer Road	U	61.5	F	57.4	F

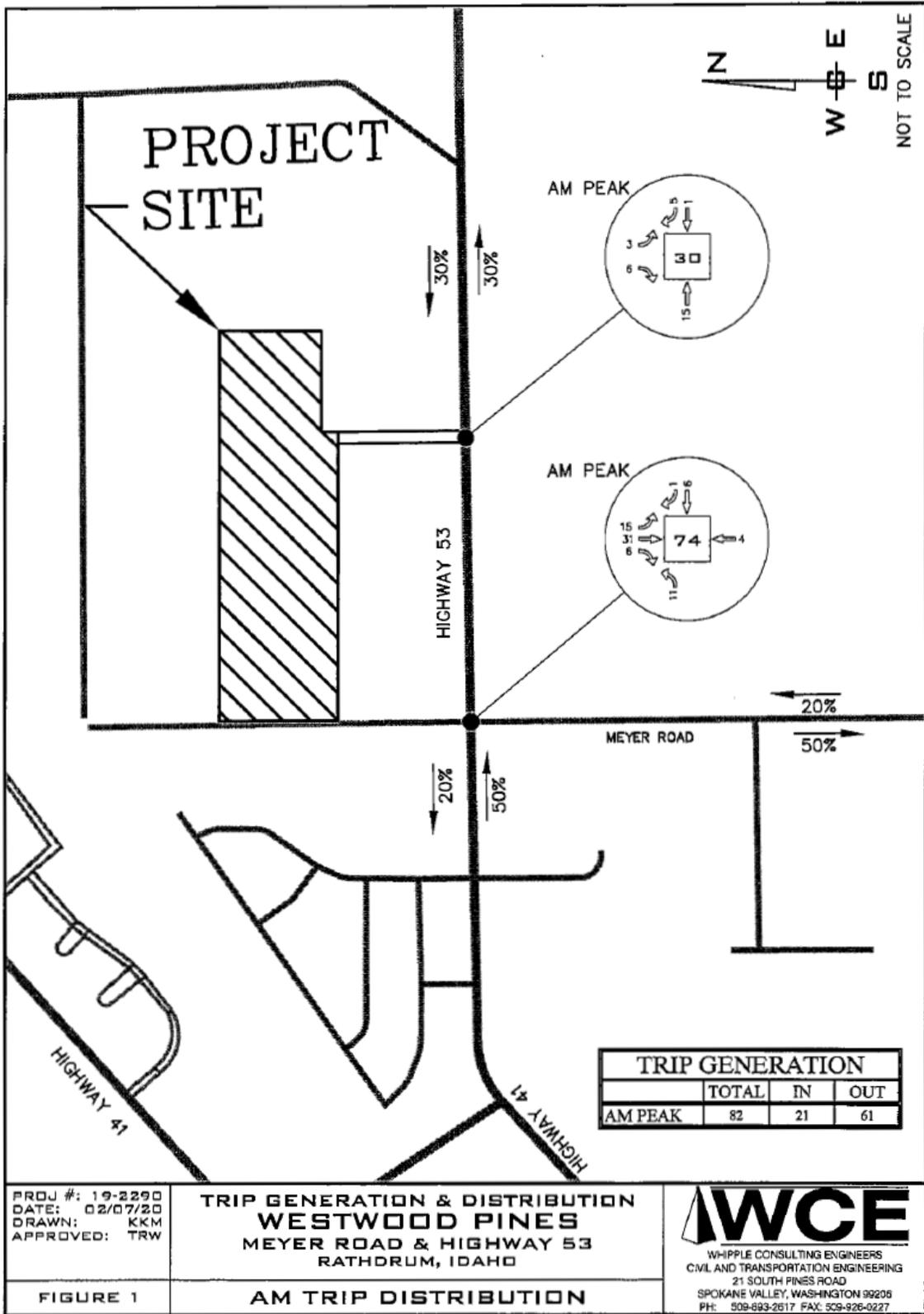
Table 3 shows that with all of the project trips applied today the intersection is anticipated to fall below an acceptable level of service, during a period of 2 hours each weekday. But the project trips are not going to occur today, they are going to be added gradually over time, and we typically see in this area a trip generation that is less than the ITE standard, so the project may not even generate as many trips nor approach the anticipated level of service at the intersection.

#### Conclusion

It is concluded that about half of the project (60 lots) could be built and occupied without causing an impact to the intersection. Thus, providing a window for the signal to be installed. It is also concluded that based upon the results of Table 3, that the City of Rathdrum's decision to install a signal at the intersection is a prudent plan and is warranted.

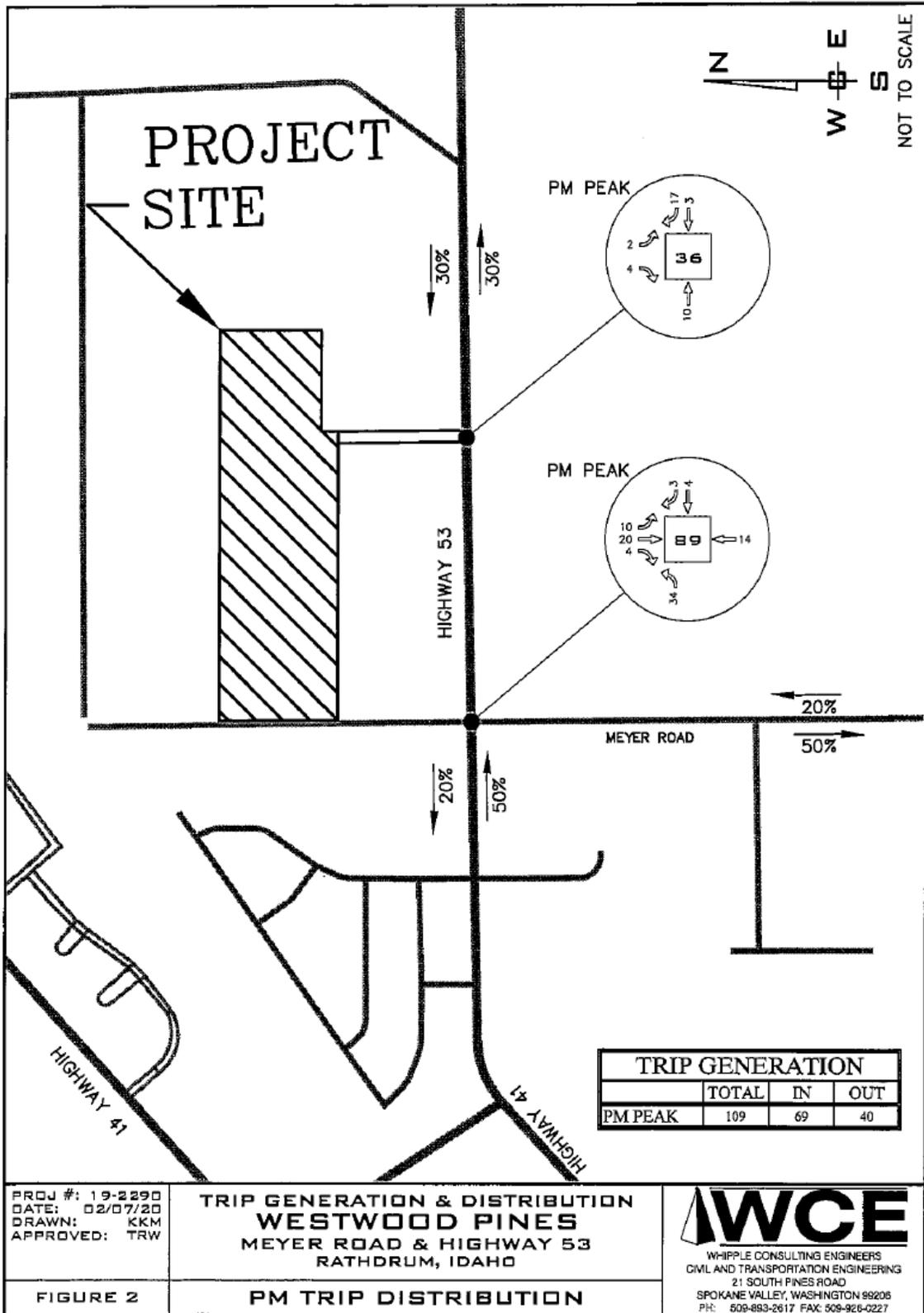
#### Recommendation

We recommend that the project continue to support the signal operation, as it is programmed within the City, through the project impact fees. Then if the signal has not been installed by the time that the project has 60 occupied homes (about 2 years). We recommend that the intersection be counted and a level of service be run to determine the remaining capacity of the intersection. And a level of service be run to determine if the remaining 50 lots would fit within that capacity at the same trip generation rate as the first 60 lots.



PROJ #: 19-2290  
 DATE: 02/07/20  
 DRAWN: KKM  
 APPROVED: TRW

**TRIP GENERATION & DISTRIBUTION**  
**WESTWOOD PINES**  
 MEYER ROAD & HIGHWAY 53  
 RATHDRUM, IDAHO



Intersection												
Int Delay, s/veh	6.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↕		↕			↕			↕	
Traffic Vol, veh/h	1	207	212	66	297	0	129	0	61	0	0	0
Future Vol, veh/h	1	207	212	66	297	0	129	0	61	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	200	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	81	81	81	81	81	81	81	81	81	81	81	81
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	256	262	81	367	0	159	0	75	0	0	0
Major/Minor	Major1	Major2			Minor1			Minor2				
Conflicting Flow All	367	0	0	518	0	0	787	256	956	1049	367	
Stage 1	-	-	-	-	-	-	258	258	-	529	529	-
Stage 2	-	-	-	-	-	-	529	529	-	427	520	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1192	-	-	1048	-	-	309	324	783	238	227	678
Stage 1	-	-	-	-	-	-	747	694	-	533	527	-
Stage 2	-	-	-	-	-	-	533	527	-	606	532	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1192	-	-	1048	-	-	286	292	783	199	205	678
Mov Cap-2 Maneuver	-	-	-	-	-	-	286	292	-	199	205	-
Stage 1	-	-	-	-	-	-	746	693	-	532	476	-
Stage 2	-	-	-	-	-	-	481	476	-	547	531	-
Approach	EB	WB			NB			SB				
HCM Control Delay, s	0	1.6			32.1			0				
HCM LOS					D			A				
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	359	1192	-	-	1048	-	-	-				
HCM Lane V/C Ratio	0.653	0.001	-	-	0.078	-	-	-				
HCM Control Delay (s)	32.1	8	0	-	8.7	0	-	0				
HCM Lane LOS	D	A	A	-	A	A	-	A				
HCM 95th %tile Q(veh)	4.4	0	-	-	0.3	-	-	-				

HCM 6th TWSC  
1: Meyer Road & Highway 53

2020 PM W/O Project  
02/07/2020

Intersection												
Int Delay, s/veh	6.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕			↕			↕	
Traffic Vol, veh/h	0	300	125	49	292	0	159	1	49	0	0	1
Future Vol, veh/h	0	300	125	49	292	0	159	1	49	0	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	200	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	323	134	53	314	0	171	1	53	0	0	1
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	314	0	0	457	0	0	744	743	323	837	877	314
Stage 1	-	-	-	-	-	-	323	323	-	420	420	-
Stage 2	-	-	-	-	-	-	421	420	-	417	467	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1246	-	-	1104	-	-	331	343	718	286	287	726
Stage 1	-	-	-	-	-	-	689	650	-	611	589	-
Stage 2	-	-	-	-	-	-	610	589	-	613	568	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1246	-	-	1104	-	-	316	323	718	253	270	726
Mov Cap-2 Maneuver	-	-	-	-	-	-	316	323	-	253	270	-
Stage 1	-	-	-	-	-	-	689	650	-	611	555	-
Stage 2	-	-	-	-	-	-	574	555	-	567	568	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0			1.2			29.6			10		
HCM LOS							D			B		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	364	1246	-	-	1104	-	-	726				
HCM Lane V/C Ratio	0.617	-	-	-	0.048	-	-	0.001				
HCM Control Delay (s)	29.6	0	-	-	8.4	0	-	10				
HCM Lane LOS	D	A	-	-	A	A	-	B				
HCM 95th %tile Q(veh)	3.9	0	-	-	0.1	-	-	0				

HCM 6th TWSC  
1: Meyer Road & Highway 53

60 Lots  
2020 AM W Project  
02/07/2020

Intersection												
Int Delay, s/veh	9.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↕		↕	↕		↕	↕		↕	↕
Traffic Vol, veh/h	7	207	212	66	300	1	129	2	61	8	16	3
Future Vol, veh/h	7	207	212	66	300	1	129	2	61	8	16	3
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	-	-	200	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	81	81	81	81	81	81	81	81	81	81	81	81
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	9	256	262	81	370	1	159	2	75	10	20	4

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	371	0	518	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	4.12	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	2.218	-	2.218	-
Pot Cap-1 Maneuver	1188	-	1048	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	1188	-	1048	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.1	1.6	43.5	25.2
HCM LOS			E	D

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	317	1188	-	-	1048	-	-	211
HCM Lane W/C Ratio	0.748	0.007	-	-	0.078	-	-	0.158
HCM Control Delay (s)	43.5	8.1	0	-	8.7	0	-	25.2
HCM Lane LOS	E	A	A	-	A	A	-	D
HCM 95th %tile Q(veh)	5.7	0	-	-	0.3	-	-	0.5

HCM 6th TWSC  
1: Meyer Road & Highway 53

2020 PM W Project  
02/07/2020

60 CoTs

Intersection												
Int Delay, s/veh	7.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↕		↕		↕	↕			↕	
Traffic Vol, veh/h	17	300	125	49	294	2	159	8	49	5	10	3
Future Vol, veh/h	17	300	125	49	294	2	159	8	49	5	10	3
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	200	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	18	323	134	53	316	2	171	9	53	5	11	3
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	318	0	0	457	0	0	789	783	323	880	916	317
Stage 1	-	-	-	-	-	-	359	359	-	423	423	-
Stage 2	-	-	-	-	-	-	430	424	-	457	493	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1242	-	-	1104	-	-	308	325	718	268	272	724
Stage 1	-	-	-	-	-	-	659	627	-	609	588	-
Stage 2	-	-	-	-	-	-	603	587	-	583	547	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1242	-	-	1104	-	-	279	300	718	229	251	724
Mov Cap-2 Maneuver	-	-	-	-	-	-	279	300	-	229	251	-
Stage 1	-	-	-	-	-	-	646	614	-	597	554	-
Stage 2	-	-	-	-	-	-	555	553	-	522	536	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.3			1.2			29.9			19.2		
HCM LOS							D			C		
Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1			
Capacity (veh/h)	279	601	1242	-	-	1104	-	-	273			
HCM Lane V/C Ratio	0.613	0.102	0.015	-	-	0.048	-	-	0.071			
HCM Control Delay (s)	36.4	11.7	7.9	0	-	8.4	0	-	19.2			
HCM Lane LOS	E	B	A	A	-	A	A	-	C			
HCM 95th %tile Q(veh)	3.7	0.3	0	-	-	0.1	-	-	0.2			

Intersection												
Int Delay, s/veh	13.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔	↔		↔			↔			↔	
Traffic Vol, veh/h	12	207	212	66	303	1	129	4	61	15	31	6
Future Vol, veh/h	12	207	212	66	303	1	129	4	61	15	31	6
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	200	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	81	81	81	81	81	81	81	81	81	81	81	81
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	15	256	262	81	374	1	159	5	75	19	38	7
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	375	0	0	518	0	0	845	823	256	994	1085	375
Stage 1	-	-	-	-	-	-	286	286	-	537	537	-
Stage 2	-	-	-	-	-	-	559	537	-	457	548	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1183	-	-	1048	-	-	283	309	783	224	217	671
Stage 1	-	-	-	-	-	-	721	675	-	528	523	-
Stage 2	-	-	-	-	-	-	513	523	-	583	517	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1183	-	-	1048	-	-	217	273	783	182	192	671
Mov Cap-2 Maneuver	-	-	-	-	-	-	217	273	-	182	192	-
Stage 1	-	-	-	-	-	-	707	662	-	518	472	-
Stage 2	-	-	-	-	-	-	421	472	-	513	507	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.2			1.6			61.5			30.2		
HCM LOS							F			D		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	282	1183	-	-	1048	-	-	206				
HCM Lane V/C Ratio	0.849	0.013	-	-	0.078	-	-	0.312				
HCM Control Delay (s)	61.5	8.1	0	-	8.7	0	-	30.2				
HCM Lane LOS	F	A	A	-	A	A	-	D				
HCM 95th %tile Q(veh)	7.2	0	-	-	0.3	-	-	1.3				

Intersection												
Int Delay, s/veh	13.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕			↕			↕	
Traffic Vol, veh/h	34	300	125	49	296	3	159	15	49	10	20	5
Future Vol, veh/h	34	300	125	49	296	3	159	15	49	10	20	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	200	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	37	323	134	53	318	3	171	16	53	11	22	5
Major/Minor	Major1	Major2			Minor1			Minor2				
Conflicting Flow All	321	0	0	457	0	0	836	824	323	925	957	320
Stage 1	-	-	-	-	-	-	397	397	-	426	426	-
Stage 2	-	-	-	-	-	-	439	427	-	499	531	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1239	-	-	1104	-	-	287	308	718	250	258	721
Stage 1	-	-	-	-	-	-	629	603	-	606	586	-
Stage 2	-	-	-	-	-	-	597	585	-	554	526	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1239	-	-	1104	-	-	245	278	718	205	233	721
Mov Cap-2 Maneuver	-	-	-	-	-	-	245	278	-	205	233	-
Stage 1	-	-	-	-	-	-	603	578	-	581	552	-
Stage 2	-	-	-	-	-	-	536	551	-	479	504	-
Approach	EB	WB			NB			SB				
HCM Control Delay, s	0.6	1.2			57.4			22.2				
HCM LOS					F			C				
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	289	1239	-	-	1104	-	-	247				
HCM Lane V/C Ratio	0.83	0.03	-	-	0.048	-	-	0.152				
HCM Control Delay (s)	57.4	8	0	-	8.4	0	-	22.2				
HCM Lane LOS	F	A	A	-	A	A	-	C				
HCM 95th %tile Q(veh)	6.9	0.1	-	-	0.1	-	-	0.5				

**From:** Scott Krajack <Scott@vikinghomes.com>  
**Sent:** Friday, February 7, 2020 6:59 AM  
**To:** Ray Kimball <rkimball@whipplece.com>  
**Cc:** Kevin Jump <kevin@rathdrum.org>; Cary Siess <cary@rathdrum.org>  
**Subject:** house construction pace in Timber Landings

Ray,

The first home we built in Timber Landings was dug on 1/20/2017. The last home we dug in Timber Landings was dug on 1/28/2020, just over 3 years later. We have dug exactly 96 homes in the last 3 years, for an average of 32 per year, or 2.67 homes per month.

If we have our Westwood Pines subdivision complete by 11/2020(earliest possible scenario), we will have our first home complete in July of 2021. Kevin believes the Highway 53 & Meyer signal will be complete by summer 2022, which would mean we should have about 32 homes built and occupied when the signal is operational.

Scott

Scott Krajack

Viking Builders, LLC | Land Development Director | [scott@vikinghomes.com](mailto:scott@vikinghomes.com)  
19425 East Broadway Avenue, Spokane Valley, WA 99016  
Office 509.241.3555 | Direct 509.960.8301 | Mobile 208.659.4833

YOUR DREAM YOUR HOUSE  
*You're Home*

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SW 1/4, SEC.29, T.52N., R.04E., B.M.

# WESTWOOD PINES PRELIMINARY SUBDIVISION PLAN LOCATED IN A PORTION OF SECTION 29, T52N, R04W CITY OF RATHDRUM, KOOTENAI COUNTY, IDAHO

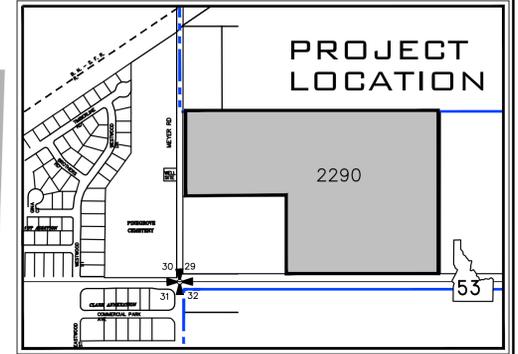
**UNDERGROUND SERVICE ALERT**  
ONE-CALL NUMBER  
**811**  
CALL TWO BUSINESS DAYS  
BEFORE YOU DIG

30 29

6' SIGHT OBSCURING FENCE TO BE CONSTRUCTED SOUTH OF NORTH PROPERTY LINE. CONSTRUCTION TO COINCIDE WITH DEVELOPMENT OF PHASES 2 AND 3.

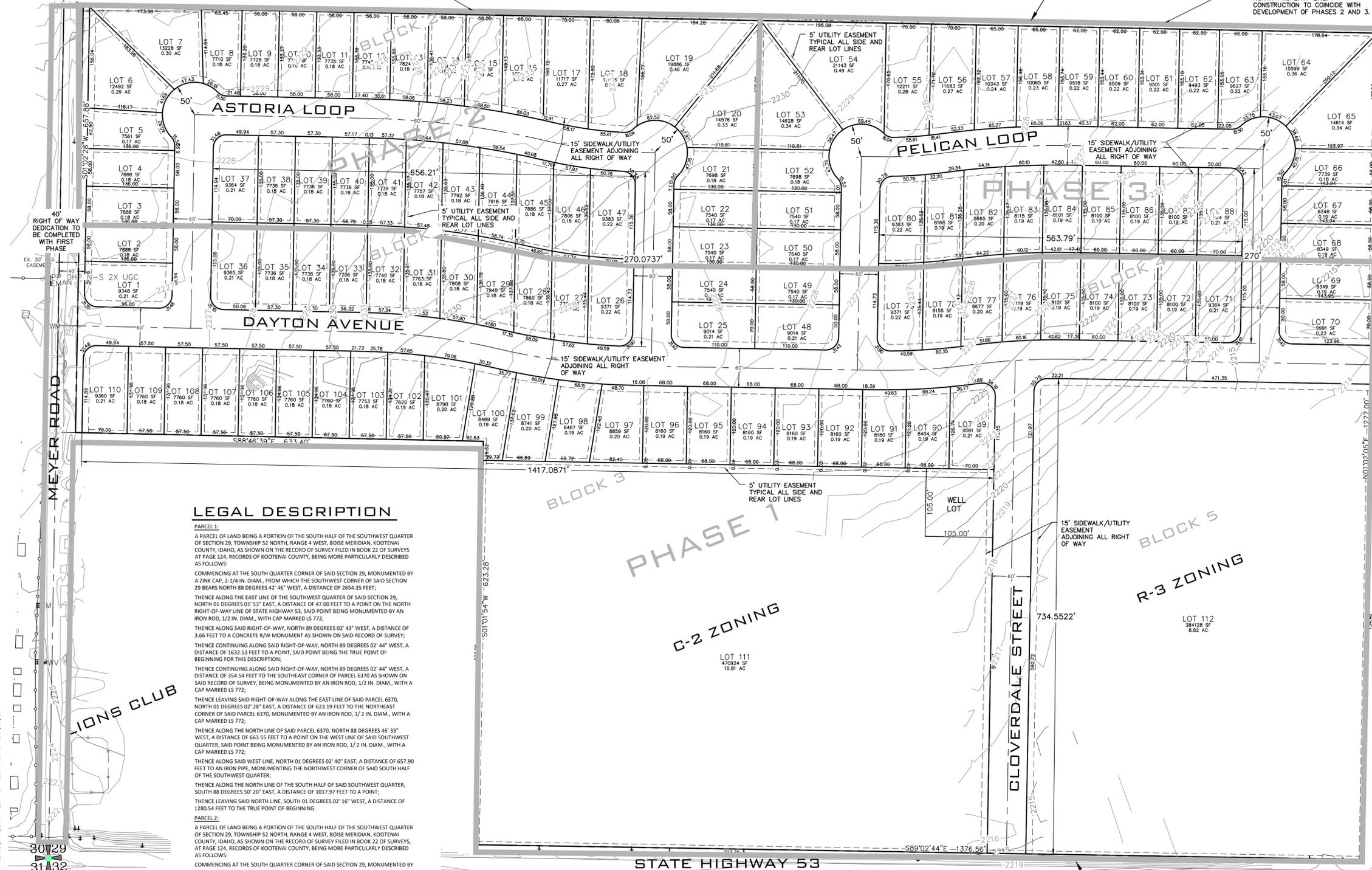
6' SIGHT OBSCURING FENCE TO BE CONSTRUCTED SOUTH OF NORTH PROPERTY LINE. CONSTRUCTION TO COINCIDE WITH DEVELOPMENT OF PHASES 2 AND 3.

CITY LIMITS



VICINITY MAP

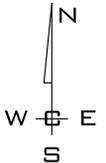
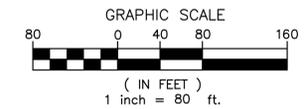
CITY LIMITS



SITE DATA TABLE	
GENERAL DESIGN DATA	
PARCEL NUMBERS	R-0000-029-6200,6500
EXISTING ZONING	R-2S, R-3, C-2
LANDUSE	VACANT
PROJECT AREA (ac)	50,0000
R-2S AREA (ac)	29.55
NUMBER OF LOTS	112
MAXIMUM LOT AREA (SF)	21,143
MINIMUM LOT AREA (SF)	7,540
PROPERTY DENSITY (lots/ac)	3.72
AVERAGE LOT AREA (SF)	8,995
DOMESTIC WATER	CITY OF RATHDRUM
SANITARY SEWER DISPOSAL	CITY OF RATHDRUM
FIRE DEPARTMENT	NORTHERN LAKES FIRE DIST
DATUM	NAVD-88
ELECTRIC PROVIDER	AVISTA
GAS PROVIDER	AVISTA
OTHER PURVEYORS	TO BE DETERMINED

### PHASING

PHASE 1: 50 SF LOTS, 1 C-2 LOT, 1 R-3 LOT COMPLETED 2020-21  
PHASE 2: 32 SF LOTS COMPLETED 2021-22  
PHASE 3: 28 SF LOTS COMPLETED 2022-23



### LEGAL DESCRIPTION

**PARCEL 1:**  
A PARCEL OF LAND BEING A PORTION OF THE SOUTH HALF OF THE SOUTHWEST QUARTER OF SECTION 29, TOWNSHIP 52 NORTH, RANGE 4 WEST, ROOSE MERIDIAN, KOOTENAI COUNTY, IDAHO, AS SHOWN ON THE RECORD OF SURVEY FILED IN BOOK 22 OF SURVEYS AT PAGE 124, RECORDS OF KOOTENAI COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:  
COMMENCING AT THE SOUTH QUARTER CORNER OF SAID SECTION 29, MONUMENTED BY A ZINC CAP, 2-1/4 IN. DIAM., FROM WHICH THE SOUTHWEST CORNER OF SAID SECTION 29 BEARS NORTH 88 DEGREES 42' 46" WEST, A DISTANCE OF 2654.35 FEET;  
THENCE ALONG THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 29, NORTH 01 DEGREES 01' 53" EAST, A DISTANCE OF 47.00 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY LINE OF STATE HIGHWAY 53, SAID POINT BEING MONUMENTED BY AN IRON ROD, 1/2 IN. DIAM., WITH CAP MARKED LS 772;  
THENCE ALONG SAID RIGHT-OF-WAY, NORTH 89 DEGREES 02' 43" WEST, A DISTANCE OF 3.66 FEET TO A CONCRETE R/W MONUMENT AS SHOWN ON SAID RECORD OF SURVEY;  
THENCE CONTINUING ALONG SAID RIGHT-OF-WAY, NORTH 89 DEGREES 02' 44" WEST, A DISTANCE OF 1632.53 FEET TO A POINT, SAID POINT BEING THE TRUE POINT OF BEGINNING FOR THIS DESCRIPTION;  
THENCE CONTINUING ALONG SAID RIGHT-OF-WAY, NORTH 89 DEGREES 02' 44" WEST, A DISTANCE OF 354.54 FEET TO THE SOUTHWEST CORNER OF PARCEL 6370 AS SHOWN ON SAID RECORD OF SURVEY, BEING MONUMENTED BY AN IRON ROD, 1/2 IN. DIAM., WITH A CAP MARKED LS 772;  
THENCE LEAVING SAID RIGHT-OF-WAY ALONG THE EAST LINE OF SAID PARCEL 6370, NORTH 01 DEGREES 02' 28" EAST, A DISTANCE OF 623.19 FEET TO THE NORTHEAST CORNER OF SAID PARCEL 6370, MONUMENTED BY AN IRON ROD, 1/2 IN. DIAM., WITH A CAP MARKED LS 772;  
THENCE ALONG THE NORTH LINE OF SAID PARCEL 6370, NORTH 88 DEGREES 46' 33" WEST, A DISTANCE OF 1663.55 FEET TO A POINT ON THE WEST LINE OF SAID SOUTHWEST QUARTER, SAID POINT BEING MONUMENTED BY AN IRON ROD, 1/2 IN. DIAM., WITH A CAP MARKED LS 772;  
THENCE ALONG SAID WEST LINE, NORTH 01 DEGREES 02' 40" EAST, A DISTANCE OF 657.90 FEET TO AN IRON PIPE, MONUMENTING THE NORTHWEST CORNER OF SAID SOUTHWEST QUARTER;  
THENCE ALONG THE NORTH LINE OF THE SOUTH HALF OF SAID SOUTHWEST QUARTER, SOUTH 88 DEGREES 50' 20" EAST, A DISTANCE OF 1017.97 FEET TO A POINT;  
THENCE LEAVING SAID NORTH LINE, SOUTH 01 DEGREES 02' 16" WEST, A DISTANCE OF 1280.54 FEET TO THE TRUE POINT OF BEGINNING.

**PARCEL 2:**  
A PARCEL OF LAND BEING A PORTION OF THE SOUTH HALF OF THE SOUTHWEST QUARTER OF SECTION 29, TOWNSHIP 52 NORTH, RANGE 4 WEST, ROOSE MERIDIAN, KOOTENAI COUNTY, IDAHO, AS SHOWN ON THE RECORD OF SURVEY FILED IN BOOK 22 OF SURVEYS, AT PAGE 124, RECORDS OF KOOTENAI COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:  
COMMENCING AT THE SOUTH QUARTER CORNER OF SAID SECTION 29, MONUMENTED BY A ZINC CAP, 2-1/4 IN. DIAM., FROM WHICH THE SOUTHWEST CORNER OF SAID SECTION 29 BEARS NORTH 88 DEGREES 42' 46" WEST, A DISTANCE OF 2654.35 FEET;  
THENCE ALONG THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 29, NORTH 01 DEGREES 01' 53" EAST, A DISTANCE OF 47.00 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY LINE OF STATE HIGHWAY 53, SAID POINT BEING MONUMENTED BY AN IRON ROD, 1/2-IN. DIAM., WITH CAP MARKED LS 772;  
THENCE ALONG SAID RIGHT-OF-WAY, NORTH 89 DEGREES 02' 43" WEST, A DISTANCE OF 3.66 FEET TO A CONCRETE R/W MONUMENT AS SHOWN ON SAID RECORD OF SURVEY;  
THENCE CONTINUING ALONG SAID RIGHT-OF-WAY, NORTH 89 DEGREES 02' 44" WEST, A DISTANCE OF 630.52 FEET TO A POINT, SAID POINT BEING THE TRUE POINT OF BEGINNING FOR THIS DESCRIPTION;  
THENCE CONTINUING ALONG SAID RIGHT-OF-WAY, NORTH 89 DEGREES 02' 44" WEST, A DISTANCE OF 1022.01 FEET TO A POINT;  
THENCE LEAVING SAID RIGHT-OF-WAY, NORTH 01 DEGREES 02' 16" EAST, A DISTANCE OF 1280.54 FEET TO A POINT ON THE NORTH LINE OF THE SOUTH HALF OF SAID SOUTHWEST QUARTER;  
THENCE ALONG THE NORTH LINE OF THE SOUTH HALF OF SAID SOUTHWEST QUARTER, SOUTH 88 DEGREES 50' 20" EAST, A DISTANCE OF 1021.94 FEET TO A POINT;  
THENCE LEAVING SAID NORTH LINE, SOUTH 01 DEGREES 02' 05" WEST, A DISTANCE OF 1276.86 FEET TO THE TRUE POINT OF BEGINNING.

BASIS OF BEARING: THE SOUTH LINE OF THE SW 1/4 OF SECTION 29, T53N, R04W, B.M.

CITY LIMITS

NAVD-88  
XXXX

NO.	DATE	BY	REVISIONS
A	1-23-19	RDK	ORIGINAL PREPARATION

<b>SCALE:</b>	<b>PROJ #:</b> 19-2290	<input checked="" type="checkbox"/> CIVIL
<b>HORIZONTAL:</b>	<b>DATE:</b> 12/06/19	<input type="checkbox"/> STRUCTURAL
<b>VERTICAL:</b>	<b>DRAWN:</b> RDK	<input type="checkbox"/> SURVEYING
1"=80'	<b>REVIEWED:</b> RDK	<input type="checkbox"/> TRAFFIC
1"=NA		<input type="checkbox"/> PLANNING
		<input type="checkbox"/> LANDSCAPE
		<input type="checkbox"/> OTHER

**WICE**  
WHIPPLE CONSULTING ENGINEERS  
21 SOUTH PINES ROAD  
SPOKANE VALLEY, WA 99206  
PH: 509-893-2617 FAX: 509-826-0227

**WESTWOOD PINES  
PRELIMINARY SUBDIVISION  
RATHDRUM, ID**

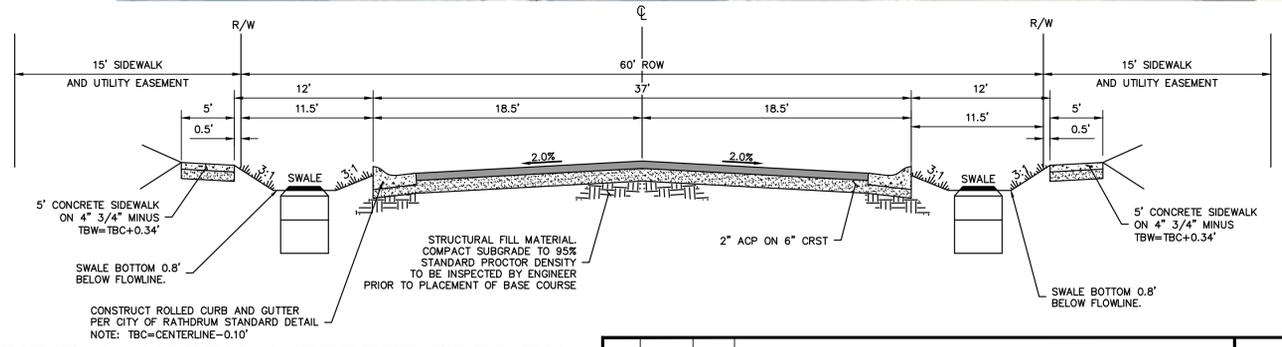
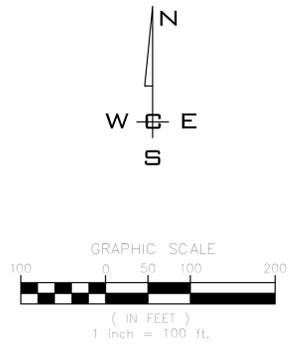
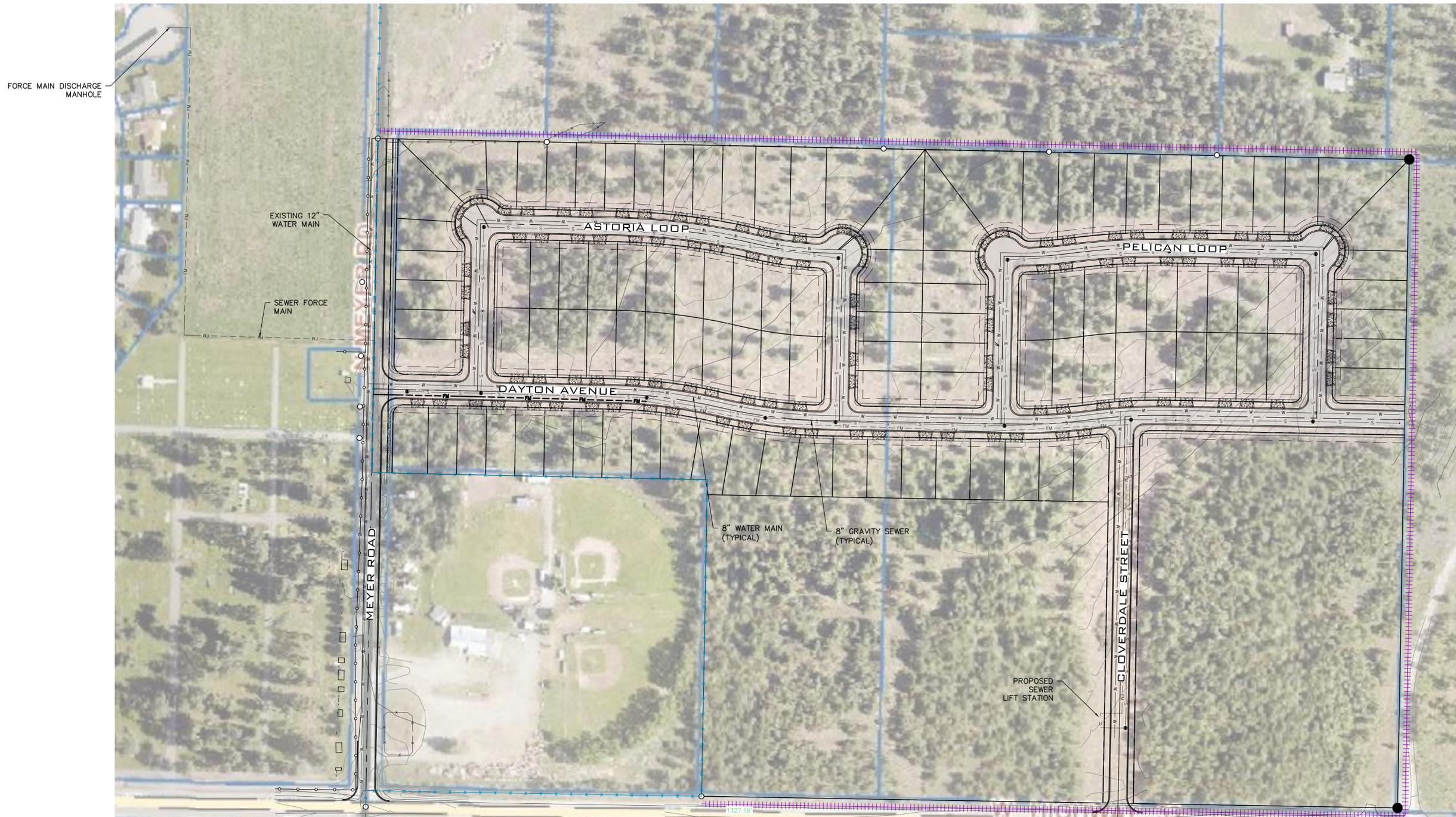
**SHEET  
1 OF 1**  
JOB NUMBER  
**19-2290**

SCOTT KRAJACK, LAND DEVELOPMENT DIRECTOR, VIKING CONSTRUCTION

WATER AND SEWER SERVICE SHALL BE PROVIDED BY THE CITY OF RATHDRUM. STORMWATER SHALL BE TREATED AND DISPOSED OF VIA SWALES AND DRYWELLS AS REQUIRED BY THE CITY OF RATHDRUM. GAS, ELECTRICITY, AND COMMUNICATIONS WILL BE PROVIDED.

**ENGINEERING (CONTACT)** THIS PRELIMINARY PLAT WAS PREPARED BY: WHIPPLE CONSULTING ENGINEERS, 21 S. PINES ROAD, SPOKANE VALLEY, WA 99216, PHONE: 208-762-9106, PHONE: 893-2617, CONTACT: RAY KIMBALL, P.E.

**OWNER** VIKING CONSTRUCTION, INC. 19425 E BROADWAY AVE SPOKANE VALLEY, WA 99016 PHONE: 208-762-9106 CONTACT: SCOTT KRAJACK



PLANS NOT APPROVED BY AGENCY

TYPICAL INTERNAL STREET SECTION

NAVD-88  
XXXX

NO.	DATE	BY	REVISIONS
A	1-23-19	RDK	ORIGINAL PREPARATION

**SCALE:**  
HORIZONTAL:  
1"=100'  
VERTICAL:  
1"=10'

PROJ #: 19-2290  
DATE: 06/12/19  
DRAWN: CTR  
REVIEWED: RDK

**WCE**  
WHIPPLE CONSULTING ENGINEERS  
21 SOUTH PINES ROAD  
SPOKANE VALLEY, WA 99206  
PH: 509-893-2617 FAX: 509-926-0227

**WESTWOOD PINES  
UTILITY DRAWING  
RATHDRUM, ID**

**SHEET  
1 OF 1**  
JOB NUMBER  
**19-2290**

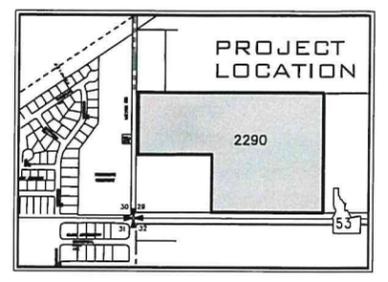
SW 1/4, SEC. 29, T. 52N., R. 04E., B.M.

# STREET & DRAINAGE PLANS

## WESTWOOD PINES PINES

LOCATED IN A PORTION OF  
SW 1/4, SEC. 29, T. 52N., R. 04 E., W.M.  
RATHDRUM, ID

UNDERGROUND SERVICE ALERT  
ONE-CALL NUMBER  
**811**  
CALL TWO BUSINESS DAYS  
BEFORE YOU DIG

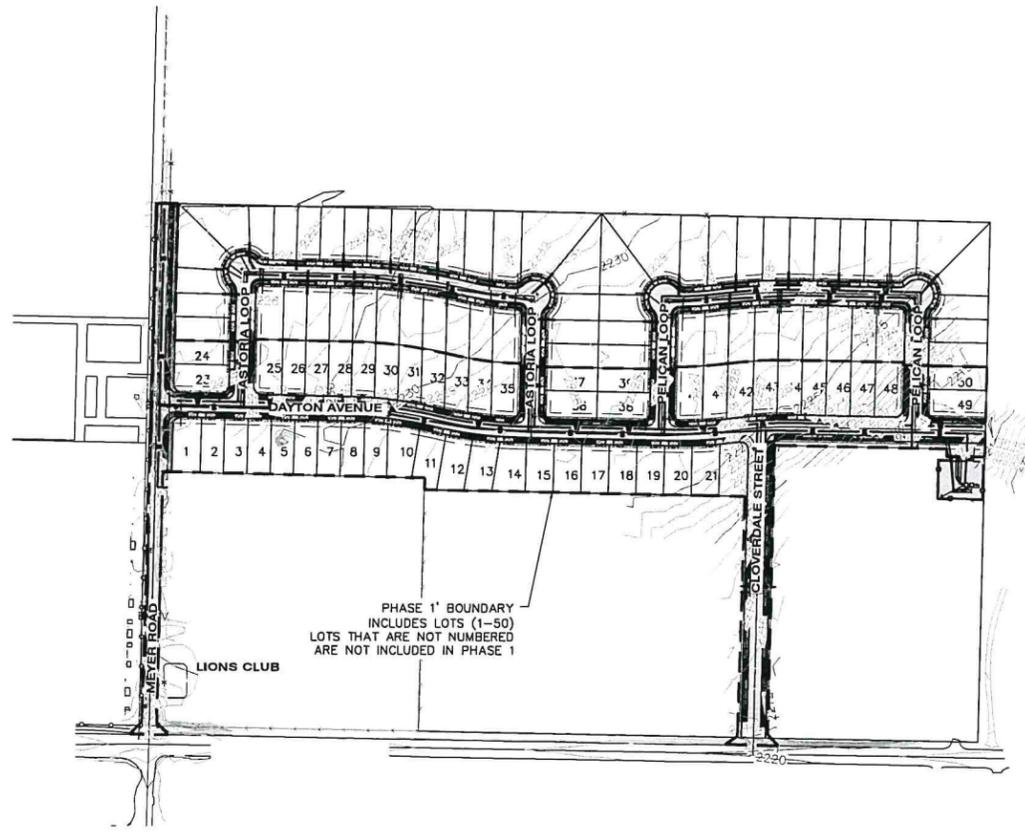


VICINITY MAP

SHEET INDEX	
C 0.0	COVER SHEET
C 0.1	GENERAL CONSTRUCTION NOTES & TYPICAL CROSS SECTIONS
C 1.0	TOPOGRAPHY (REFERENCE ONLY)
C 3.0	MEYER ROAD PLAN AND PROFILE
C 3.1	DAYTON AVENUE PLAN AND PROFILE
C 3.2	DAYTON AVENUE PLAN AND PROFILE CONT.
C 3.3	ASTORIA & PELICAN LOOP PLAN AND PROFILE
C 3.4	CLOVERDALE STREET PLAN AND PROFILE
C 3.5	INTERSECTION DETAILS
C 3.6	LIFT STATION PLAN
C 3.7	LIFT STATION DETAILS
C 3.8	LIFT STATION DETAILS CONT.
C 3.9	LIFT STATION DETAILS CONT.
C 4.0	GRADING PLAN
C 5.0	STORM OVERVIEW
C 5.1	EROSION CONTROL PLAN
C 7.0	UTILITY OVERVIEW
C 9.0	SLEEVING, LIGHTING & SIGNAGE PLAN
C 10.0	CONSTRUCTION DETAILS
C 10.1	CONSTRUCTION DETAILS CONTINUED

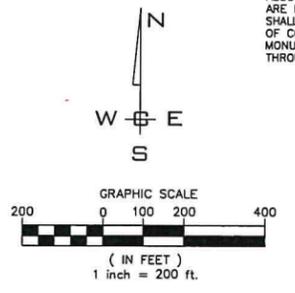
EXISTING	DESCRIPTION	PROPOSED
---	ROADWAY CENTER LINE	---
---	RIGHT OF WAY LINE	---
---	PROPERTY LINE	---
---	EASEMENT LINE	---
-x-x-	FENCE	-x-x-
---	CURB	---
---	PAVEMENT	---
---	GRAVEL	---
---	CONCRETE	---
	BUILDINGS & STRUCTURES	
●	MONUMENT	●
<b>SEWER</b>		
-S-S-S-	SANITARY SEWER	-S-S-S-
-S-S-S-	MANHOLE	-S-S-S-
-S-S-S-	CLEANOUT	-S-S-S-
-S-S-S-	SEWER SERVICE	-S-S-S-
<b>WATER</b>		
-W-W-W-	WATER LINE	-W-W-W-
-W-W-W-	VALVE	-W-W-W-
-W-W-W-	FIRE HYDRANT SERVICE	-W-W-W-
-W-W-W-	WATER METER	-W-W-W-
-W-W-W-	BLOWOFF	-W-W-W-
-W-W-W-	AIR VACUUM RELIEF STATION	-W-W-W-
<b>DRAINAGE</b>		
-D-D-D-	DRAINAGE LINE	-D-D-D-
-D-D-D-	MANHOLE	-D-D-D-
-D-D-D-	DRYWELL	-D-D-D-
-D-D-D-	CATCH BASIN	-D-D-D-
-D-D-D-	DITCH	-D-D-D-
<b>GAS</b>		
-G-G-G-	GAS LINE	-G-G-G-
-G-G-G-	VALVE	-G-G-G-
-G-G-G-	METER	-G-G-G-
<b>TELE-POWER</b>		
-BT-BT-BT-	BURIED TELEPHONE	-BT-BT-BT-
-PP-PP-PP-	POWER OR TELEPHONE POLE	-PP-PP-PP-
-BP-BP-BP-	BURIED POWER	-BP-BP-BP-
-BP-BP-BP-	TRANSFORMER PAD	-BP-BP-BP-
-BP-BP-BP-	TELEPHONE RISER	-BP-BP-BP-
-OP-OP-OP-	TELEPHONE VAULT	-OP-OP-OP-
-OP-OP-OP-	OVERHEAD POWER	-OP-OP-OP-
-OP-OP-OP-	GUY ANCHOR	-OP-OP-OP-
-OP-OP-OP-	POWER VAULT	-OP-OP-OP-
-OP-OP-OP-	LIGHT POLE	-OP-OP-OP-

ABBREVIATIONS	
ACT. LEN.	ACTUAL LENGTH
BCR	BEGINNING OF CURVE RADIUS
BDRY.	BOUNDARY
CO.	SEWER CLEANOUT
CSTC	CRUSHED SURFACE TOP COURSE
CT.	COURT
DIA.	DIAMETER
ECR	END OF CURVE RADIUS
EXIST.	EXISTING
G	GRADE
GB	GRADE BREAK
FT./FT.	FEET PER FOOT
HYD.	HYDRANT
I.E.	INVERT ELEVATION
LN.	LANE
LT.	LEFT
MH	MANHOLE
MCR	MIDDLE OF CURVE RADIUS
PC	POINT OF CURVATURE
PET.	PETROLEUM
PI	POINT OF INTERSECTION
PRC	POINT OF REVERSE CURVE
PT	POINT OF TANGENCY
RIM EL.	RIM ELEVATION
RD	ROAD
RT.	RIGHT
SI	STREET INTERSECTION
SS	SANITARY SEWER
STA.	STATION
STA. LEN.	STATION LENGTH
TG	TOP OF GRATE
TC	TOP OF CURB



SITE PLAN  
SCALE: 1"=200'

SPECIAL NOTE  
AVAILABLE RECORDS AND FIELD RESEARCH HAS BEEN CONDUCTED, TO LOCATE EXISTING PROPERTY CORNERS ASSOCIATED WITH THE PROJECT. ALL EXISTING MONUMENTS ARE IDENTIFIED WITHIN THE PLANS, AND THESE LOCATIONS SHALL BE VERIFIED PRIOR TO, AND AT THE COMPLETION OF CONSTRUCTION, PER IDAHO CODE 55-1613. EXISTING MONUMENT SHALL BE PRESERVED AND PROTECTED THROUGHOUT THE COURSE OF CONSTRUCTION.



DEVELOPER'S APPROVAL \_\_\_\_\_ DATE \_\_\_\_\_

- |   |  |  |   |
|---|--|--|---|
| <b>PUBLIC WORKS</b><br>CITY OF RATHDRUM<br>8047 W. MAIN STREET<br>RATHDRUM, IDAHO 83858<br>PHONE: 1-687-2700, X118<br>CONTACT: KEVIN JUMP, P.E.         | <b>FIRE</b><br>NORTHERN LAKES F.P.D.<br>125 WEST HAYDEN AVENUE<br>HAYDEN, IDAHO 83835<br>PHONE: 1-208-442-5711<br>CONTACT: TYLER DRECHSEL                | <b>BUILDING</b><br>CITY OF RATHDRUM<br>8047 WEST MAIN STREET<br>RATHDRUM, IDAHO 83858<br>PHONE: 1-208-687-2700 X125<br>CONTACT: SHERMAN HUNTER         | <b>HEALTH</b><br>PANHANDLE HEALTH DISTRICT<br>8500 NORTH ATLAS ROAD<br>HAYDEN, IDAHO 83835<br>PHONE: 1-208-415-5200<br>CONTACT: PHYLLIS SELLMER |
| <b>GAS / POWER</b><br>AVSTA UTILITIES<br>1735 15TH STREET<br>COEUR D'ALENE, IDAHO 83814<br>PHONE: 1-208-769-1871<br>CONTACT: JAMIE HOWARD               | <b>PHONE</b><br>FRONTIER COMMUNICATIONS<br>747 W. PRAIRIE AVENUE<br>COEUR D'ALENE, IDAHO 83835<br>PHONE: 1-800-921-8101<br>CONTACT: RON HELMITK          | <b>CABLE</b><br>CHARTER COMMUNICATIONS<br>COEUR D'ALENE, IDAHO 83835<br>PHONE: 1-208-769-8332<br>CONTACT: JASON W NEBEL                                | <b>SOLID WASTE</b><br>WASTE MANAGEMENT<br>PHONE: 1-866-909-4458   |
| <b>LANDSCAPE</b><br>WHIPPLE CONSULTING ENGINEERS<br>21 SOUTH PINES ROAD<br>SPOKANE VALLEY, WA 99206<br>PHONE: 509-893-2617<br>CONTACT: SUSAN MOSS, ASLA | <b>SURVEYOR</b><br>WHIPPLE CONSULTING ENGINEERS<br>21 SOUTH PINES ROAD<br>SPOKANE VALLEY, WA 99206<br>PHONE: 509-893-2617<br>CONTACT: JON GORDON, P.L.S. | <b>ENGINEERING</b><br>WHIPPLE CONSULTING ENGINEERS<br>21 S PINES ROAD<br>SPOKANE VALLEY, WA 99206<br>PHONE: 509-893-2617<br>CONTACT: RAY KIMBALL, P.E. | <b>OWNER</b><br>VIKING CONSTRUCTION<br>2505 WEST HAYDEN AVENUE<br>HAYDEN, IDAHO 83877<br>PHONE: 1-208-489-6100<br>CONTACT: SCOTT KRALACK        |

P:\MAIL\WORK\2018\1801\PROJECTS\2018-2290\WESTWOOD PINES\2018-2290-001\CONSTR\DWG\181217Z18

NAVD-88  
XXXX

SCALE: HORIZONTAL: 1"=200' VERTICAL: N/A		PROJ #: 19-2290 DATE: 12/06/19 DRAWN: XXX REVIEWED: RDR	<p>WHIPPLE CONSULTING ENGINEERS 21 SOUTH PINES ROAD SPOKANE VALLEY, WA 99206 PH: 509-893-2617 FAX: 509-925-0227</p>	<p><b>WESTWOOD PINES</b> <b>COVER SHEET</b> <b>RATHDRUM, ID</b></p>	<p><b>SHEET</b> <b>C0.0</b></p> <p>JOB NUMBER <b>19-2290</b></p>															
<table border="1"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>ORIGINAL PREPARATION</th> <th>REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>		NO.	DATE	BY	ORIGINAL PREPARATION	REVISIONS						<table border="1"> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </table>								
NO.	DATE	BY	ORIGINAL PREPARATION	REVISIONS																



**GENERAL**

- ALL WORK SHALL CONFORM TO THE "IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION," (ISPCW) CURRENT EDITION, AND THE ASSOCIATED HIGHWAY DISTRICT STANDARDS.
- NO REVISIONS SHALL BE MADE TO THESE PLANS WITHOUT THE APPROVAL OF THE CITY ENGINEER. ALL PROPOSED REVISIONS SHALL BE SUBMITTED BY THE ENGINEER OF RECORD FOR REVIEW AND APPROVAL BY THE CITY.
- ALL SAFETY STANDARDS AND REQUIREMENTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR, AND COMPLIED WITH AS SET FORTH BY OSHA.
- EXISTING UTILITIES SHALL BE LOCATED BY CONTACTING CALL BEFORE YOU DIG AT 811, AT LEAST 48 HOURS PRIOR TO STARTING ANY EXCAVATIONS. THE CONTRACTOR SHALL NOTIFY THE APPROPRIATE UTILITY COMPANIES PRIOR TO STARTING WORK NEAR ANY FACILITIES, AND SHALL COORDINATE THEIR WORK WITH COMPANY REPRESENTATIVES.
- WORK SHALL NOT BEGIN UNTIL A PERMIT AND NOTICE TO PROCEED IS ISSUED BY THE CITY.
- THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEERING INSPECTOR AT LEAST 48 HOURS PRIOR TO STARTING WORK, OR PROCEEDING WITH NEW PHASES OF CONSTRUCTION. ALL INSPECTIONS SHALL BE SCHEDULED WITH A MINIMUM 48-HOUR NOTICE PRIOR TO TESTING.
- AN APPROVED SET OF IMPROVEMENT PLANS SHALL BE KEPT ON THE JOB SITE AT ALL TIMES.
- THE CONTRACTOR SHALL MAINTAIN THE STREETS, SIDEWALKS, AND ALL OTHER PUBLIC RIGHTS-OF-WAY IN A CLEAN, SAFE AND USEABLE CONDITION. ALL SOIL, ROCK, OR CONSTRUCTION DEBRIS SHALL BE PROPERLY REMOVED FROM THE PUBLICLY OWNED PROPERTY DURING CONSTRUCTION, AND UPON COMPLETION OF THE PROJECT, ALL ADJACENT PROPERTY, PRIVATE OR PUBLIC, SHALL BE MAINTAINED IN A CLEAN, SAFE AND USEABLE CONDITION.
- EXISTING PROPERTY CORNERS OR SURVEY MONUMENTS SHALL BE PROTECTED DURING THE COURSE OF CONSTRUCTION. ANY DAMAGED OR OBLITERATED CORNERS OR MONUMENTS SHALL BE RE-ESTABLISHED BY PROFESSIONAL SURVEYORS, LICENSED TO WORK IN THE STATE OF IDAHO, PRIOR TO FINAL ACCEPTANCE.
- TREES NOT IDENTIFIED FOR REMOVAL SHALL BE PRESERVED OR PROTECTED IN AN APPROVED MANNER PRIOR TO COMMENCEMENT OF GRADING OPERATIONS.
- ALL CONCRETE, UNLESS OTHERWISE SPECIFIED, SHALL BE COMMERCIAL GRADE PORTLAND CEMENT WITH AIR ENTRAINMENT (6.5% +/- 1.5%), AND A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 3000 PSI.
- ALL UNDERGROUND UTILITY LATERALS SHALL BE INSTALLED AND APPROVED BEFORE CONSTRUCTION OF CURBS, CROSS GUTTERS, SIDEWALKS OR THE SURFACING OF STREETS.
- ALL TRENCHES WITHIN PUBLIC EASEMENTS OR RIGHTS-OF-WAY SHALL BE COMPACTED IN ACCORDANCE WITH ISPCW. COMPACTION TEST RESULTS SHALL BE CERTIFIED BY THE ENGINEER OF RECORD, AND SUBMITTED TO THE CITY ENGINEER FOR APPROVAL PRIOR TO ANY PAVING AND FINAL ACCEPTANCE OF WORK.
- ALL OPERATIONS CONDUCTED ON THE PREMISES SHALL BE RESTRICTED TO THE HOURS BETWEEN 7:00 A.M. AND 10:00 P.M., MONDAY THROUGH FRIDAY, UNLESS OTHERWISE APPROVED BY THE CITY. THIS INCLUDES THE WARMING UP, REPAIR, ARRIVAL, DEPARTURE OR RUNNING OF TRUCKS, EARTHMOVING EQUIPMENT, CONSTRUCTION EQUIPMENT OR ANY OTHER ASSOCIATED EQUIPMENT.
- ALL EXISTING IMPROVEMENTS SHALL BE JOINED OR MATCHED IN A MANNER SATISFACTORY TO THE CITY ENGINEER. THIS INCLUDES ALL UTILITY CONNECTIONS AND NECESSARY SAW CUTTING, REMOVAL, REPLACEMENT AND CAPPING ASSOCIATED WITH CURB AND GUTTER, SIDEWALKS, SWALES, ASPHALT, CONCRETE OR OTHER PAVING.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL, IN ACCORDANCE WITH THE I.U.T.C.D. CURRENT EDITION. AT LEAST 24 HOURS PRIOR TO DISRUPTION OF ANY TRAFFIC, TRAFFIC CONTROL PLANS SHALL BE PREPARED AND SUBMITTED TO THE CITY ENGINEERING DIVISION FOR APPROVAL. NO WORK SHALL COMMENCE UNTIL A PERMIT IS ISSUED AND ALL APPROVED TRAFFIC CONTROL IS IN PLACE.
- ALL PERMANENT ROADWAY PATCHING SHALL BE PLACED WITHIN 7 DAYS OF THE EXCAVATION, EXCEPT IN THE AREAS THAT ARE PART OF A LARGER PROJECT AND ACTIVE WORK ZONE. A TEMPORARY PATCH UTILIZING A MINIMUM OF 2 INCHES OF ASPHALT CONCRETE (COLD MIX) SHALL BE PLACED THE SAME DAY AS THE EXCAVATION. TEMPORARY STREET PATCHING EXCEEDING 7 DAYS MAY BE ALLOWED FOR, AS APPROVED BY THE CITY ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL PATCHING.

**STORM SEWER & SANITARY SEWER DRAINAGE NOTES**

- TEMPORARY EROSION CONTROL AND WATER POLLUTION MEASURES SHALL BE REQUIRED AND INSTALLED, IN ACCORDANCE WITH THE PLANS OR ACCEPTED BEST MANAGEMENT PRACTICES. AT NO TIME, WILL SILTS AND/OR DEBRIS BE ALLOWED TO DRAIN INTO AN EXISTING OR NEWLY INSTALLED FACILITY.
- SWALES WITHIN AREAS OF MASS GRADING SHALL BE SCARIFIED A MINIMUM OF 24 INCHES PRIOR TO SHAPING, AND AFTER INSTALLATION OF CURB AND GUTTER.
- ALL DISTURBED AREAS SHALL RECEIVE A MINIMUM 1-INCH DRESSING OF TOP SOIL AND BE HYDRO SEEDED OR SOODED, AS INDICATED ON THE PLANS. SEEDS AREAS WILL NOT BE ACCEPTED UNTIL THE SEED HAS GERMINATED, AND THE GRASS IS THOROUGHLY ESTABLISHED. SOODED AREAS WILL NOT BE ACCEPTED UNTIL THE ROOTS HAVE TAKEN HOLD, AND THE GRASS HAS RECEIVED TWO CUTTINGS.
- CARE SHALL BE TAKEN TO PREVENT COMPACTION OF THE SUB-GRADE IN THE GRASS INFILTRATION AREAS OF SWALES. IN THE EVENT THE SUB-GRADE SHOULD BE COMPACTED OR INSUFFICIENT PERCOLATION IS OBSERVED, TESTING OF THE SUB-GRADE MAY BE REQUIRED AT THE DISCRETION OF THE CITY ENGINEER. IF A SUFFICIENT PERCOLATION IS NOT OBSERVED, THE SUB-GRADE MUST BE REMOVED AND REPLACED, OR SCARIFIED TO A DEPTH OF 24" AND RETESTED.
- TOPSOIL PLACED WITHIN THE SWALES SHALL BE FREE DRAINING, AND PLACED AT A DEPTH GREATER THAN 1-INCH AND LESS THAN 3-INCHES. AT CONCRETE SPILLWAYS, FINISHED TOP SOIL SHALL BE KEPT 1"-2" BELOW THE FINISHED CONCRETE SURFACE. TO PREVENT COMPACTION OF THE SUB-GRADE AND TOPSOIL, WHEELED EQUIPMENT SHOULD NOT BE USED WITHIN THE SWALE AREA. THE MINIMUM PERCOLATION RATE THROUGH A CONSTRUCTED SWALE SHALL MEET DESIGN REQUIREMENTS. TESTING OF PERCOLATION RATES THROUGH A CONSTRUCTED SWALE MAY BE REQUIRED AT THE DISCRETION OF THE CITY ENGINEER.
- DRYWELLS SHALL BE INSTALLED TO THE ELEVATIONS INDICATED ON THE PLANS. THE ELEVATION OF THE DRYWELL RIM SHALL BE AT LEAST 0.2 FEET BELOW LOWEST ADJOINING CURB CUT. FINISHED TOP SOIL ADJACENT TO THE DRYWELL SHALL BE AT LEAST 2-INCHES BELOW THE DRYWELL RIM.
- GRASS INFILTRATION AREAS SHALL BE HYDRO SEEDED WITH 50 LB. / 1,000 SQUARE FEET, CONSISTING OF A MIXTURE WITH EQUAL PORTIONS OF CANADA BLUEGRASS, CRESTED WHEATGRASS, HARD FESCUE AND SHEEP FESCUE. SEEDS AREAS SHALL BE FERTILIZED WITH A COMMERCIAL FERTILIZER PER THE MANUFACTURERS SPECIFICATIONS AND MULCHED WITH "SILVA FIBER PLUS", OR APPROVED EQUAL WOOD FIBER CELLULOSE AT A RATE OF 1 TON PER ACRE.
- ALL SANITARY SEWER MAINS SHALL BE AIR TESTED IN ACCORDANCE WITH ISPCW, SECTION 501, AND VIDEO INSPECTED IN THE PRESENCE OF THE ENGINEER OF RECORD AND THE CITY ENGINEERING INSPECTOR.
- STORM SEWER PIPES AND DRYWELLS SHALL BE SEPARATED AT LEAST 10 FEET HORIZONTALLY FROM DOMESTIC WATER MAINS. CROSSINGS OF WATER MAINS AND SEWER SYSTEMS SHALL HAVE A MINIMUM 18-INCH VERTICAL SEPARATION. ANY ANTICIPATED SEPARATION LESS THAN MINIMUM STANDARDS CONTAINED HEREIN, SHALL CONFORM TO THE IDAHO RULES FOR PUBLIC DRINKING WATER SYSTEMS (DAPA 58.01.16).

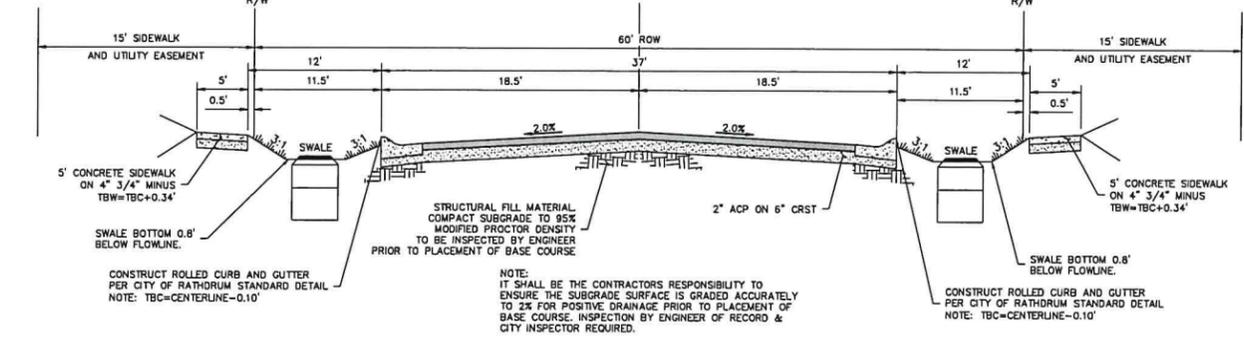
**STREETS**

- ALL FILL PLACED WITHIN THE ROADWAY PRISM SHALL BE COMPACTED TO 90% OF THE MODIFIED PROCTOR, WITH THE EXCEPTION OF THE TOP 24 INCHES OF SUB-GRADE THAT SHALL BE COMPACTED TO 95% OF THE MODIFIED PROCTOR (ASTM D-1557).
- PRIOR TO PLACING BASE MATERIAL, THE FOLLOWING SHALL BE COMPLETED:
  - ALL PUBLIC UTILITIES SHALL BE INSTALLED, TESTED AND APPROVED.
  - THE LINE AND GRADE OF THE SUB-GRADE SHALL BE INSPECTED AND APPROVED BY THE ENGINEER OF RECORD AND THE CITY ENGINEERING INSPECTOR. RED TOPS ARE REQUIRED FOR INSPECTION.
  - A PROOF-ROLL OF THE SUB-GRADE SHALL BE PERFORMED AND OBSERVED BY THE ENGINEER OF RECORD AND CITY ENGINEERING INSPECTOR.
  - OBTAIN AUTHORIZATION FROM THE ENGINEER OF RECORD AND CITY ENGINEERING INSPECTOR, TO PROCEED WITH PLACEMENT OF BASE MATERIAL. THE CITY ENGINEERING INSPECTOR SHALL BE NOTIFIED AT LEAST 24 HOURS PRIOR TO PLACEMENT OF BASE MATERIAL.
- CRUSHED AGGREGATE BASE SHALL CONFORM TO THE IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION, SECTION 802, (TYPE 1) 3/4-INCH MAXIMUM AGGREGATE SIZE, AND SHALL BE COMPACTED TO THE FOLLOWING SPECIFICATIONS:
  - ROADWAY: 95%-MODIFIED PROCTOR.
  - CURB BASE AND DRIVEWAY APPROACHES: 92%-MODIFIED PROCTOR.
  - SIDEWALKS OR TRAILS: 82%-MODIFIED PROCTOR.
- PRIOR TO PLACING ASPHALT CONCRETE, THE FOLLOWING SHALL BE COMPLETED:
  - ALL UTILITIES SHALL BE ADJUSTED TO GRADE AND ASPHALT COLLARS INSTALLED.
  - THE LINE AND GRADE OF THE BASE COURSE SHALL BE INSPECTED AND APPROVED BY THE ENGINEER OF RECORD AND THE CITY ENGINEERING INSPECTOR. BLUE TOPS ARE REQUIRED FOR INSPECTION.
  - OBTAIN AUTHORIZATION FROM THE ENGINEER OF RECORD AND CITY ENGINEER TO PROCEED WITH ASPHALT PAVING. THE CITY ENGINEERING INSPECTOR SHALL BE NOTIFIED AT LEAST 24 HOURS PRIOR TO PLACEMENT OF ASPHALT PAVEMENT.
- ASPHALT PAVEMENT SHALL CONFORM WITH IDAHO TRANSPORTATION DEPARTMENT (ITD) SPECIFICATIONS FOR SUPERPAVE. FOR PAVEMENTS 3 INCHES OR LESS, 4" COMPACTED THICKNESS; A SP3 PG 64-28 WITH 1/2" MAX AGGREGATE SIZE, OR PAVEMENTS WITH A DESIGN COMPACTED THICKNESS GREATER THAN 3 INCHES, AN INITIAL 2 INCH LIFT, COMPACTED DEPTH, OF SP3 PG 64-28 WITH 3/4" MAX AGGREGATE SIZE SHALL BE PLACED; WITH THE REMAINDER OF THE PAVEMENT SURFACE BEING SP3 PG 64-28 WITH 1/2" MAX AGGREGATE SIZE.
- NO ASPHALT SHALL BE PLACED ON WET OR FROZEN SURFACES, OR WHEN THE AIR OR GROUND TEMPERATURE IS LESS THAN 40F. TOP COURSES OR PAVEMENT THICKNESS LESS THAN 2.5 INCHES SHALL NOT BE PLACED WHEN AIR OR GROUND TEMPERATURE IS LESS THAN 50F, WITHOUT APPROVAL BY THE CITY ENGINEER.
- A TACK COAT SHALL BE APPLIED TO ALL ADJACENT CURBS AND JOINTS, PRIOR TO PLACEMENT OF ASPHALTIC CONCRETE.
- DURING PAVING OPERATIONS, THE ENGINEER OF RECORD SHALL OBSERVE PAVING OPERATIONS, AND PERFORM COMPACTION AND QUALITY CONTROL TESTING.
- THE CITY ENGINEER MAY REQUIRE THE PAVEMENT SECTIONS SHOWN ON THE PLANS TO BE VERIFIED BY "R" VALUE TESTS TAKEN FROM EXPOSED SUB-GRADE.
- EXTRACTION AND GRADATION TESTS MAY BE REQUIRED AT THE DISCRETION OF THE CITY ENGINEER. ADDITIONALLY, CORING OF THE ASPHALT PAVEMENT MAY BE REQUESTED TO VERIFY PAVEMENT THICKNESS AND/OR COMPACTION.
- FORMS, SUB-GRADE AND STRING-LINE INSPECTION IS REQUIRED PRIOR TO POURING CONCRETE. A MINIMUM NOTICE OF 24 HOURS IS REQUIRED PRIOR TO INSPECTION AND SHALL BE DONE BY THE ENGINEER OF RECORD AND THE CITY ENGINEERING INSPECTOR.
- CONCRETE SHALL NOT BE PLACED ON FROZEN SURFACES, ICE OR SNOW, OR SURFACES WITH A TEMPERATURE GREATER THAN 90F. UNLESS OTHERWISE AUTHORIZED BY THE CITY ENGINEER, CONCRETE PLACEMENT SHALL BE DISCONTINUED WHEN AIR TEMPERATURES REACH 35F AND FALLING.
- CURB AND GUTTER SHALL BE CONSTRUCTED WITH FULL DEPTH CONSTRUCTION EXPANSION JOINTS ADJACENT TO CATCH BASINS, AND AT ALL RETURNS. WEAKENED PLANE JOINTS ARE REQUIRED EVERY 10 FEET. DRAINAGE APRONS ARE REQUIRED TO BE SEPARATE POURS AFTER CURBING IS CONSTRUCTED.
- SIDEWALKS SHALL BE CONSTRUCTED WITH FULL DEPTH EXPANSION JOINTS EVERY 20 FEET, AND WEAKENED PLANE JOINTS EVERY 5 FEET. JOINTS IN THE SIDEWALK SHALL BE ALIGNED WITH CURB JOINTS, AS NEARLY AS PRACTICAL.

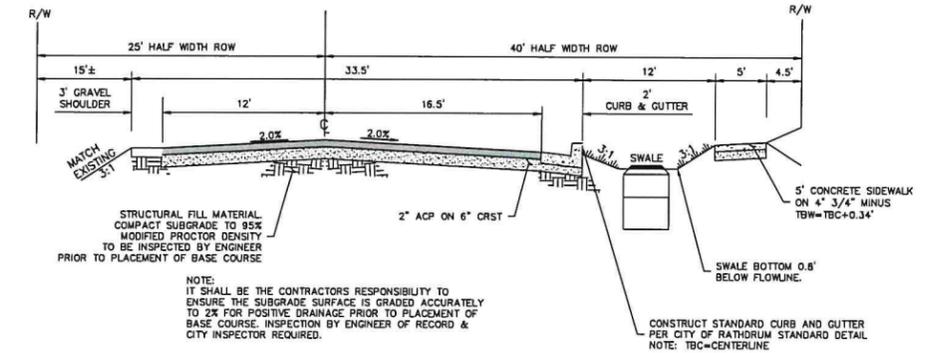
**WATER NOTES**

- WATER MAINS UP TO 12 INCHES IN DIAMETER SHALL BE PVC AWWA C900-07, OR 25 PIPE. WATER MAINS GREATER THAN 12 INCHES IN DIAMETER SHALL UTILIZE 90S, OR 25 PIPE. ALL WATER MAINS SHALL BE CONSTRUCTED WITH THE TOP OF PIPE 4.5 FEET BELOW FINISH GRADE, EXCEPT WHERE OTHERWISE INDICATED WITH SPECIFIC ELEVATIONS AND APPROVED BY THE CITY ENGINEER. WATER SERVICES SHALL BE CONSTRUCTED OF POLYETHYLENE TUBING CONFORMING TO AWWA C901, 250 PSI LOCATED AND SIZED AS SHOWN ON THE CONSTRUCTION PLANS.
- 48 HOURS PRIOR TO DISRUPTION, SHUT-OFF OR CONNECTION TO EXISTING WATER SYSTEMS, THE CONTRACTOR SHALL INFORM AND RECEIVE APPROVAL FROM THE CITY ENGINEERING DIVISION. ANY CONNECTION POINTS SHALL BE EXPOSED AT LEAST 24 HOURS PRIOR TO CONNECTION, TO VERIFY LOCATION AND FITTING REQUIREMENTS.
- AT LEAST 24 HOURS PRIOR TO SHUT-OFF, THE CONTRACTOR SHALL NOTIFY ALL AFFECTED PROPERTIES UTILIZING FLYERS, DOOR HANGERS OR LETTERS. NOTIFICATIONS SHALL CONTAIN AT A MINIMUM: DATE AND TIME OF SHUTOFF, ANTICIPATED DURATION, CONTRACTOR'S NAME AND PHONE NUMBER, AND AN EMERGENCY CONTACT PERSON AND PHONE NUMBER. A COPY OF THE NOTIFICATION SHALL BE PROVIDED TO THE CITY ENGINEERING DIVISION.
- NO CONNECTIONS FOR THE PURPOSE OF OBTAINING WATER SUPPLY DURING CONSTRUCTION SHALL BE MADE, WITHOUT FIRST OBTAINING A PERMIT FROM THE CITY WATER DIVISION.
- ALL WATER MAINS AND SERVICES SHALL BE INSTALLED WITH CONTINUOUS TRACER WIRE AND PLASTIC MARKER TAPE. TRACER WIRE SHALL BE TESTED PRIOR TO SUB-GRADE APPROVAL.
- ALL WATER MAINS SHALL BE SEPARATED AT LEAST TEN FEET HORIZONTALLY FROM SANITARY OR STORM SEWER SYSTEMS. CROSSING OF WATER MAINS AND SEWER SYSTEMS SHALL HAVE A MINIMUM 18-INCH VERTICAL SEPARATION, WITH THE FULL UNLIFT STICK OF WATER MAIN BEING CENTERED OVER THE SEWER. ANY ANTICIPATED SEPARATION DIFFERING FROM THE MINIMUM STANDARDS CONTAINED HEREIN, SHALL CONFORM TO THE IDAHO RULES FOR PUBLIC DRINKING WATER (DAPA 58.01.08).
- MAXIMUM LIMITS OF WATER MAIN JOINT AND PIPE DEFLECTION SHALL BE OBTAINED FROM THE MANUFACTURER, AND SUBMITTED TO THE ENGINEER OF RECORD AND CITY ENGINEER PRIOR TO CONSTRUCTION, WHERE LAYING CONDITIONS REQUIRE. PIPE JOINT DEFLECTION IN EXCESS OF LIMITS SPECIFIED BY THE MANUFACTURER, A THRUST BLOCKED FITTING SHALL BE USED.
- ALL THRUST BLOCKING SHALL BE FORMED IN PLACE AGAINST UNDISTURBED OR COMPACTED SOIL, AND CONFORM TO THE MINIMUM DIMENSIONS SHOWN IN SD 403. THE USE OF PRE-CAST THRUST BLOCKS IS PROHIBITED. ALL BOLTS AND NUTS SHALL BE FREE OF CONCRETE AND ACCESSIBLE BY WRENCH.
- ALL WATER MAINS, FIRE MAINS, VALVES, FIRE HYDRANTS, SERVICES AND APPURTENANCES SHALL BE INSTALLED, TESTED AND APPROVED, IN ACCORDANCE WITH REFERENCED STANDARDS IN THE GENERAL NOTES AND CITY REQUIREMENTS. (ISPCW, 150 PSI, 2-HOUR MINIMUM IN THE PRESENCE OF THE ENGINEER OF RECORD AND CITY ENGINEERING INSPECTOR PRIOR TO PAVING. RESULTS OF CHLORINATION AND BACTERIA TESTS SHALL BE SUBMITTED AND APPROVED BY THE CITY ENGINEERING DIVISION AND CITY WATER DIVISION, PRIOR TO OPERATING VALVES TO PUT NEW MAINS INTO SERVICE.

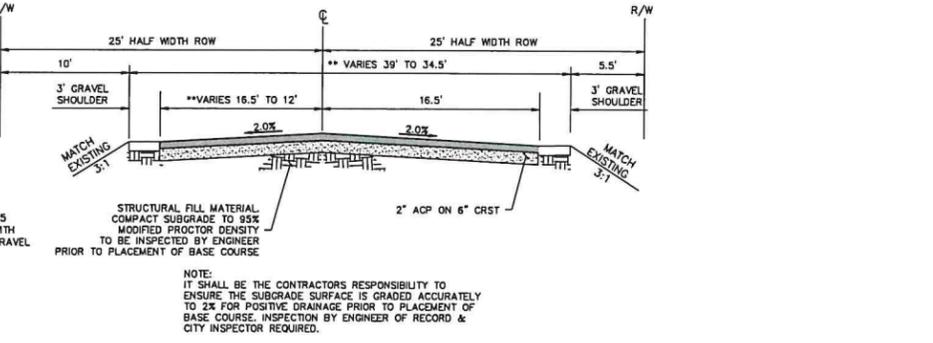
**SW 1/4, SEC. 29, T. 52 N., R. 04 E., B. M.**



**DAYTON AVE., ASTORIA LOOP, PELICAN LOOP, CLOVERDALE ST. TYPICAL SECTION**  
NOT TO SCALE



**MEYER ROAD TYPICAL SECTION STA. 11+74 TO STA. 18+28**  
NOT TO SCALE



**MEYER ROAD TYPICAL SECTION STA. 5+00 TO STA. 11+74**  
NOT TO SCALE

**WASTEWATER NOTES**

- LOCATION AND ELEVATION OF EXISTING FACILITIES SHOULD BE CONFIRMED BY FIELD MEASUREMENTS AND EXCAVATION EXPLORATION BY THE CONTRACTOR, PRIOR TO BEGINNING OF NEW WORK. ALL WORK IS TO CONFORM TO OSHA REQUIREMENTS.
- RESERVED
- EACH LOT SHALL BE SERVED BY MINIMUM OF A SINGLE FOUR (4) INCH (RESIDENTIAL), OR SIX (6) INCH (COMMERCIAL) SEWER LATERAL SET AT A MINIMUM OF 2.0% SEE STD. DWG. SD-311A. ALL SEWER LATERALS SHALL BE SHOWN ON AS-BUILT PLANS WITH STATIONING AND ELEVATIONS.
- SANITARY SEWER LATERALS SHALL BE AT RIGHT ANGLES TO PUBLIC SEWER MAINS, EXCEPT IN A CUL-DE-SAC WHERE THE (90) RULE IS NOT PRACTICAL. SEWER SERVICE CONNECTIONS SHALL BE TEES FOR ALL PUBLIC MAINS.
- THE CONTRACTOR MUST SECURE APPROVAL FROM THE CITY'S ENGINEERING INSPECTOR PRIOR TO BACKFILL OVER SEWER TEES AND LATERALS.
- NO DISRUPTION OF EXISTING SANITARY SEWER SERVICE WILL BE ALLOWED WITHOUT THE SPECIFIC APPROVAL OF THE CITY'S ENGINEER.
- THE ENGINEER OF RECORD SHALL NOTIFY THE CITY'S ENGINEERING INSPECTOR AND WASTEWATER UTILITY AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO COMMENCING WORK ON ANY PUBLIC SEWERS.
- ALL PUBLIC SEWER MAINS AND SEWER LATERALS SHALL BE PLACED, INSPECTED, TESTED, AND APPROVED BY THE CITY PRIOR TO PAVING.
- ALL PUBLIC SANITARY SEWER LINES SHALL BE CCTV AND SUBMITTED ON CD IN A USABLE FORMATTED TEMPLATE USING "DRAIN" OR ANOTHER PRE-APPROVED EQUIVALENT PROGRAM TO THE CITY'S WASTEWATER UTILITY FOR REVIEW AND APPROVAL PRIOR TO PAVING. PLEASE ALLOW FORTY-EIGHT (48) HOURS FOR REVIEW.
- ALL SANITARY MANHOLES SHALL BE INSPECTED BY THE CITY INSPECTOR, FIRST INSPECTION PRIOR TO PAVING AND SECOND INSPECTION PRIOR TO ACCEPTANCE OF IMPROVEMENTS.
- ALL SANITARY SEWER MAINS SHALL BE AIR TESTED IN ACCORDANCE WITH IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION (ISPCW). TESTING SHALL BE WITNESSED, RECORDED, AND SIGNED BY THE ENGINEER OF RECORD AND THE CITY INSPECTOR, AND THE TEST RESULTS SUBMITTED TO THE CITY'S ENGINEERING INSPECTOR.

NAVD-88  
XXXX

SCALE:	
HORIZONTAL:	N/A
VERTICAL:	N/A

NO.	DATE	BY	REVISIONS
A	1-23-18	RDK	ORIGINAL PREPARATION

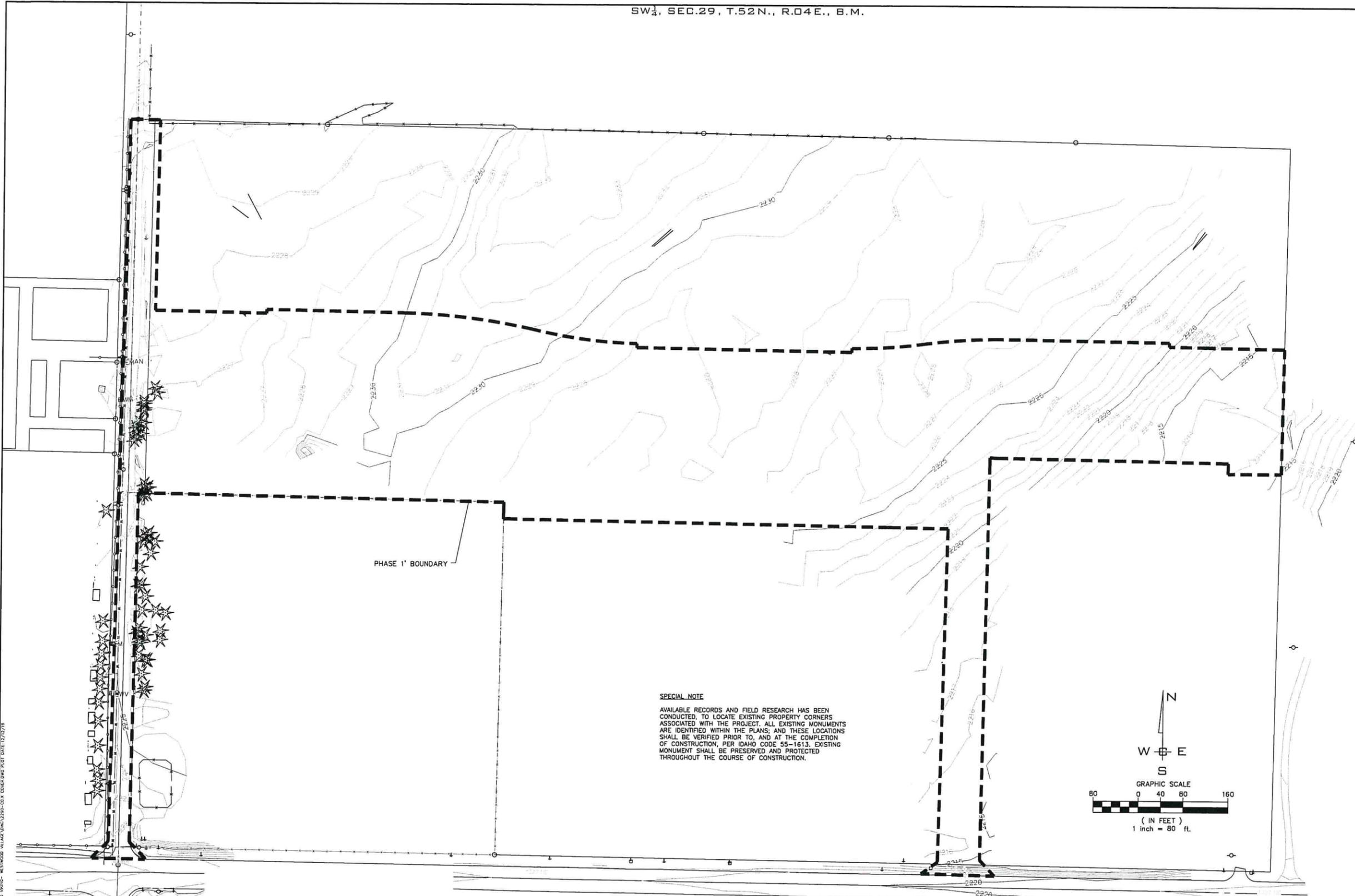
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DATE: 12/06/19  
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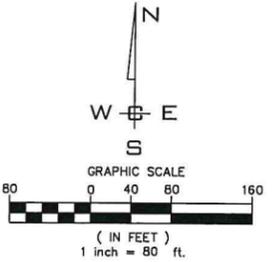
**WESTWOOD PINES**  
**GENERAL NOTES & CROSS SECTION**  
**RATHDRUM, ID**

**SHEET**  
**CO.1**  
**JOB NUMBER**  
**19-2290**

SW 1/4, SEC. 29, T. 52 N., R. 04 E., B.M.



**SPECIAL NOTE**  
 AVAILABLE RECORDS AND FIELD RESEARCH HAS BEEN CONDUCTED TO LOCATE EXISTING PROPERTY CORNERS ASSOCIATED WITH THE PROJECT. ALL EXISTING MONUMENTS ARE IDENTIFIED WITHIN THE PLANS, AND THESE LOCATIONS SHALL BE VERIFIED PRIOR TO, AND AT THE COMPLETION OF CONSTRUCTION, PER IDAHO CODE 55-1613. EXISTING MONUMENT SHALL BE PRESERVED AND PROTECTED THROUGHOUT THE COURSE OF CONSTRUCTION.



TOPOGRAPHIC SURVEY-FOR REFERENCE ONLY  
 SCALE: 1"=50'

NAVD-88  
 XXXX

FOR REFERENCE ONLY

NO.	DATE	BY	REVISIONS
A	1-23-19	RDK	ORIGINAL PREPARATION

**SCALE:**  
 HORIZONTAL:  
 1" = 80'  
 VERTICAL:  
 N/A

PROJ #: 19-2290  
 DATE: 12/06/19  
 DRAWN: CTR  
 REVIEWED: RDK

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<input type="checkbox"/>	SURVEYING
<input type="checkbox"/>	TRAFFIC
<input type="checkbox"/>	PLANNING
<input type="checkbox"/>	LANDSCAPE
<input type="checkbox"/>	OTHER

**WCE**  
 WHIPPLE CONSULTING ENGINEERS  
 21 SOUTH PINES ROAD  
 SPOKANE VALLEY, WA 99206  
 PH: 509-893-2617 FAX: 509-926-0227

**WESTWOOD PINES**  
**TOPOGRAPHICAL SURVEY (REF. ONLY)**  
**RATHDRUM, ID**

**SHEET**  
**C1.0**  
 JOB NUMBER  
**19-2290**

FILE: \\WORK\03171 INC. PROJECTS\03171-2290 - WESTWOOD PINES\03171-2290-001 - COVER SHEET.PLOT DATE: 12/17/19



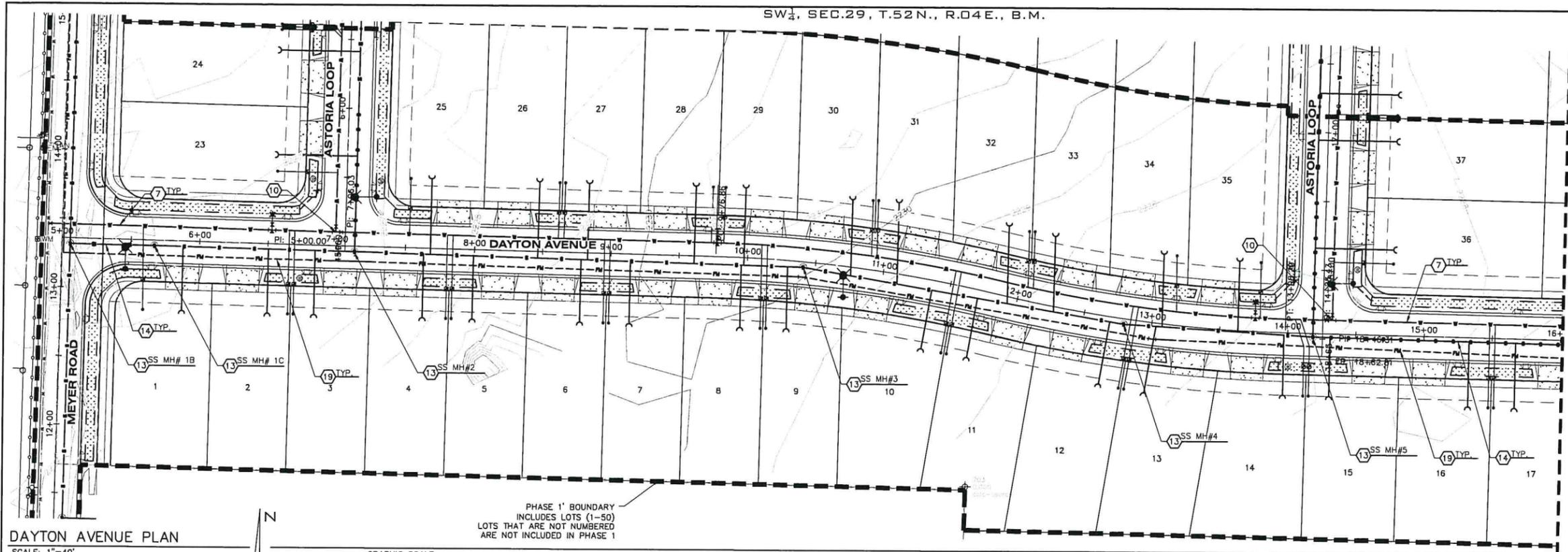
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**CONSTRUCTION NOTES**

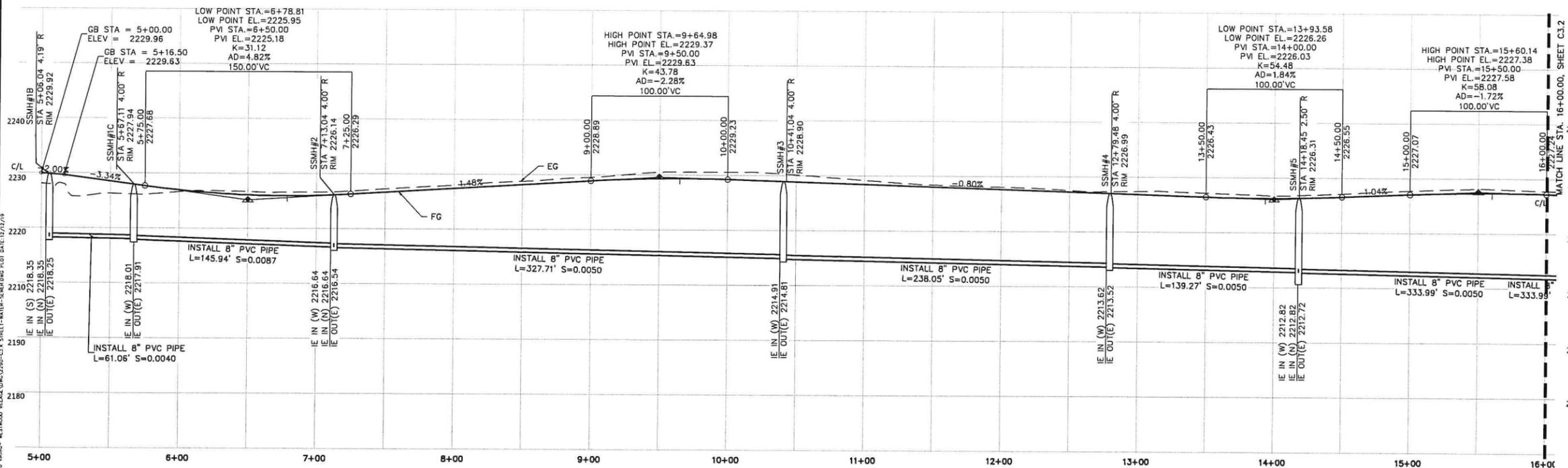
- 1 PROVIDE AND INSTALL 5' CONCRETE SIDEWALK PER CITY OF RATHDRUM STANDARD PLAN.
- 2 PROVIDE AND INSTALL CURB INLET PER CITY OF RATHDRUM STANDARD PLAN.
- 3 PROVIDE AND INSTALL ROLLED CURB PER CITY OF RATHDRUM STANDARD PLAN.
- 4 PROVIDE AND INSTALL ROAD SECTION PER DETAIL ON SHEET C0.2.
- 5 PROVIDE AND INSTALL 24"-WIDE CONCRETE DRIVEWAY APPROACH PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS.
- 6 RESERVED.
- 7 PROVIDE AND INSTALL 12" DR-18 C-900 PVC WATER MAIN PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS. 5' MINIMUM BURY.
- 8 PROVIDE AND INSTALL WATER SERVICE PER CITY OF RATHDRUM STANDARD PLAN.
- 9 PROVIDE AND INSTALL 90" MJ X MJ DI ELBOW & THRUST BLOCK PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS.
- 10 PROVIDE AND INSTALL 12" D.I. TEE W/ GATE VALVES (WHERE SHOWN ON PLAN) & THRUST BLOCK, WITH A 12" X 8" REDUCER ON NORTH BRANCH PRIOR TO GATE VALVE-PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS.
- 11 PROVIDE AND INSTALL FIRE HYDRANT ASSEMBLY W/ THRUST BLOCK PER CITY OF RATHDRUM.
- 12 PROVIDE AND INSTALL 4" SEWER SERVICE PER CITY OF RATHDRUM STANDARD PLAN.
- 13 PROVIDE AND INSTALL 48" TYPE A MANHOLE PER ISPPVC STANDARD PLAN SD-501. SEE PROFILE VIEW THIS SHEET FOR STATIONING AND INVERTS.
- 14 PROVIDE AND INSTALL 8" PVC SEWER MAIN PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS. SEE PROFILE VIEW THIS SHEET FOR LENGTH, SLOPE, AND INVERT.
- 15 PROVIDE AND INSTALL 4" SCH 80 PVC PIPE FOR IRRIGATION. CAP BOTH SIDES OF PIPE.
- 16 PROVIDE AND INSTALL PEDESTRIAN RAMP PER STANDARD DRAWING SD-712A, SHEET C10.0, AND PER THESE PLANS.
- 17 PROVIDE AND INSTALL 11.25" MJ X MJ BEND PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS.
- 18 RESERVED.
- 19 PROVIDE AND INSTALL 6" C-900 PVC FORCE MAIN PER THE CITY RATHDRUM STANDARDS.

ADDITIONAL NOTE TO CONTRACTOR: ALL WATER MAIN TEES, BENDS/ELBOWS, VALVES, AND REDUCERS REQUIRE THRUST BLOCKING PER THE CITY OF RATHDRUM'S STANDARD PLAN SD-403 ON SHEET C10.0. CONTRACTOR IS RESPONSIBLE FOR PROVIDING, DETERMINING THE AMOUNT OF THRUST BLOCKING REQUIRED, AND INSTALLING ALL REQUIRED THRUST BLOCKING PER THE CITY OF RATHDRUM'S STANDARDS.



**DAYTON AVENUE PLAN**  
SCALE: 1"=40'

PHASE 1' BOUNDARY INCLUDES LOTS (1-50) LOTS THAT ARE NOT NUMBERED ARE NOT INCLUDED IN PHASE 1



**DAYTON AVENUE PROFILE**

H SCALE: 1"=40'  
V SCALE: 1"=10'

NAVD-88  
XXXX

NO.	DATE	BY	ORIGINAL PREPARATION	REVISIONS

**SCALE:**  
HORIZONTAL:  
1" = 40'  
VERTICAL:  
1" = 10'

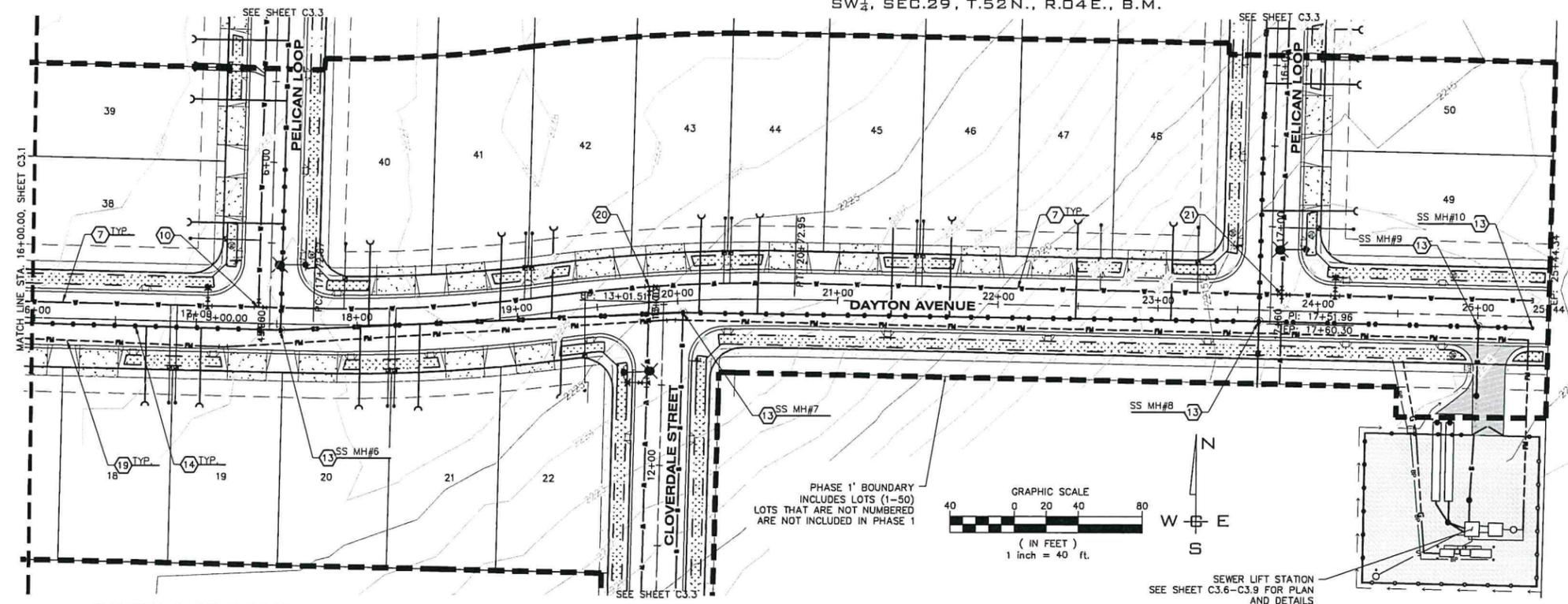
PROJ #: 19-2290  
DATE: 12/06/19  
DRAWN: CTR  
REVIEWED: RDK

**WCE**  
WHIPPLE CONSULTING ENGINEERS  
21 SOUTH PINES ROAD  
SPOKANE VALLEY, WA 99208  
PH: 509-893-2617 FAX: 509-926-0227

**WESTWOOD PINES  
DAYTON AVENUE P & P  
RATHDRUM, ID**

**SHEET  
C3.1**  
JOB NUMBER  
**19-2290**

SW 1/4, SEC.29, T.52N., R.04E., B.M.

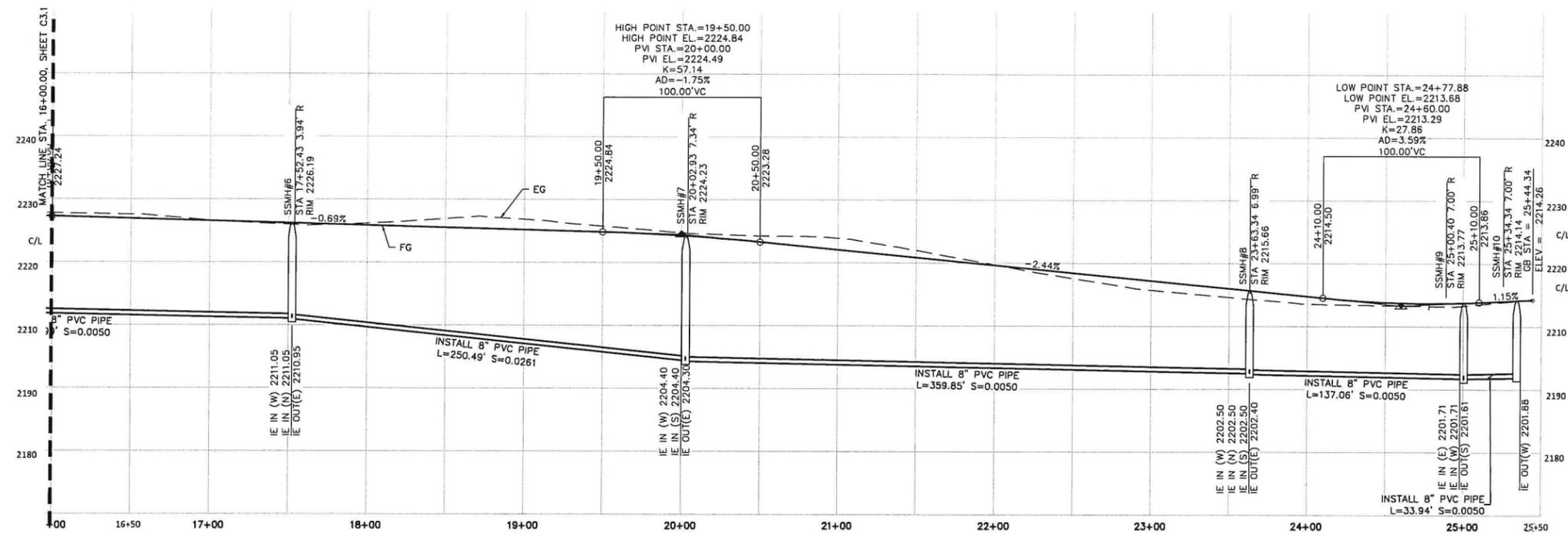


DAYTON AVENUE PLAN  
SCALE: 1"=40'

**CONSTRUCTION NOTES**

- 1 PROVIDE AND INSTALL 5' CONCRETE SIDEWALK PER CITY OF RATHDRUM STANDARD PLAN.
- 2 PROVIDE AND INSTALL CURB INLET PER CITY OF RATHDRUM STANDARD PLAN.
- 3 PROVIDE AND INSTALL ROLLED CURB PER CITY OF RATHDRUM STANDARD PLAN.
- 4 PROVIDE AND INSTALL ROAD SECTION PER DETAIL ON SHEET CO.2.
- 5 PROVIDE AND INSTALL 24'-WIDE CONCRETE DRIVEWAY APPROACH PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS.
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- 12 PROVIDE AND INSTALL 4" SEWER SERVICE PER CITY OF RATHDRUM STANDARD PLAN.
- 13 PROVIDE AND INSTALL 48" TYPE A MANHOLE PER ISWPC STANDARD PLAN SD-501. SEE PROFILE VIEW THIS SHEET FOR STATIONING AND INVERTS.
- 14 PROVIDE AND INSTALL 8" PVC SEWER MAIN PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS. SEE PROFILE VIEW THIS SHEET FOR LENGTH, SLOPE, AND INVERT.
- 15 PROVIDE AND INSTALL 4" SCH 80 PVC PIPE FOR IRRIGATION. CAP BOTH SIDES OF PIPE.
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- 17 PROVIDE AND INSTALL 11.25' MJ X MJ BEND PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS.
- 18 RESERVED.
- 19 PROVIDE AND INSTALL 6" C-900 PVC FORCE MAIN PER THE CITY RATHDRUM STANDARDS.
- 20 PROVIDE AND INSTALL 12" D.I. TEE W/ GATE VALVES (WHERE SHOWN ON PLAN) & THRUST BLOCK, PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS.
- 21 PROVIDE AND INSTALL 12" D.I. CROSS W/ GATE VALVES (WHERE SHOWN ON PLAN) & THRUST BLOCK, PER CITY OF RATHDRUM STANDARDS AND SPECIFICATIONS.

ADDITIONAL NOTE TO CONTRACTOR: ALL WATER MAIN TEES, BENDS/ELBOWS, VALVES, AND REDUCERS REQUIRE THRUST BLOCKING PER THE CITY OF RATHDRUM'S STANDARD PLAN SD-403 ON SHEET C10.0. CONTRACTOR IS RESPONSIBLE FOR PROVIDING, DETERMINING THE AMOUNT OF THRUST BLOCKING REQUIRED, AND INSTALLING ALL REQUIRED THRUST BLOCKING PER THE CITY OF RATHDRUM'S STANDARDS.



DAYTON AVENUE PROFILE  
H SCALE: 1"=40'  
V SCALE: 1"=10'

NAVD-88  
XXXX

NO.	DATE	BY	ORIGINAL PREPARATION	REVISIONS
A	1-23-19	RDK	ORIGINAL PREPARATION	

**SCALE:**  
HORIZONTAL:  
1" = 40'  
VERTICAL:  
1" = 10'

PROJ #: 19-2290  
DATE: 12/06/19  
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REVIEWED: RDK

**WCE**  
WHIPPLE CONSULTING ENGINEERS  
21 SOUTH PINES ROAD  
SPOKANE VALLEY, WA 99206  
PH: 509-893-2617 FAX: 509-825-0227

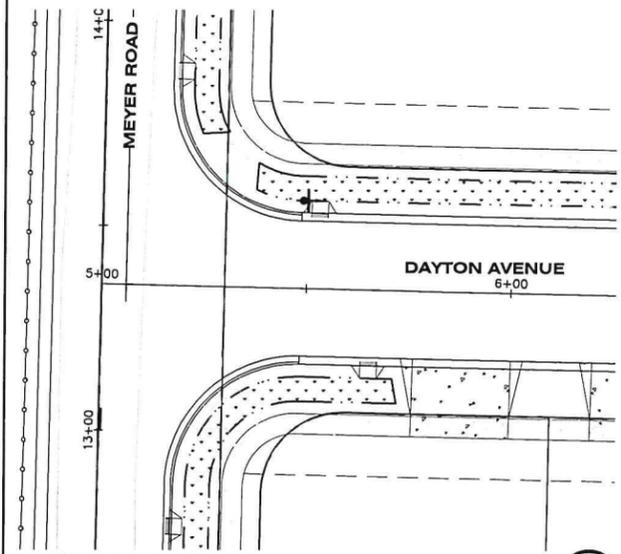
**WESTWOOD PINES  
DAYTON AVENUE P & P  
RATHDRUM, ID**

**SHEET  
C3.2**  
JOB NUMBER  
**19-2290**



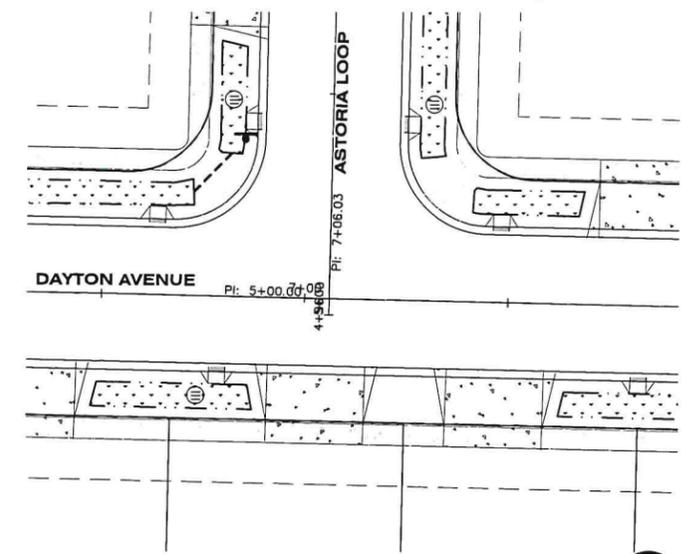


SW 1/4, SEC.29, T.52N., R.04E., B.M.



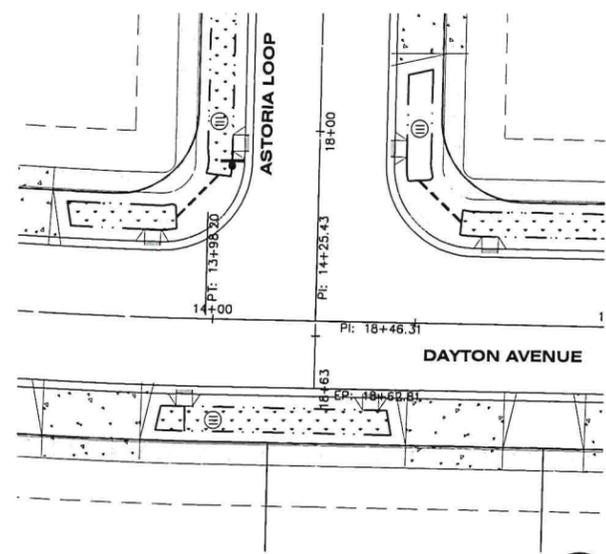
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MEYER ROAD & DAYTON AVENUE  
SCALE: 1"=20'

1



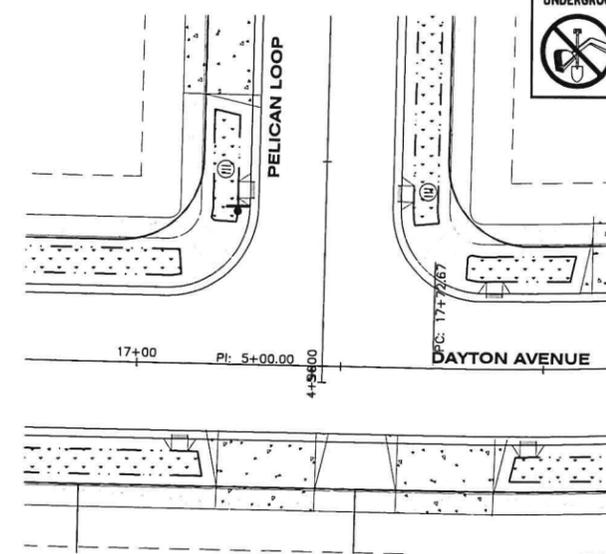
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ASTORIA LOOP & DAYTON AVENUE

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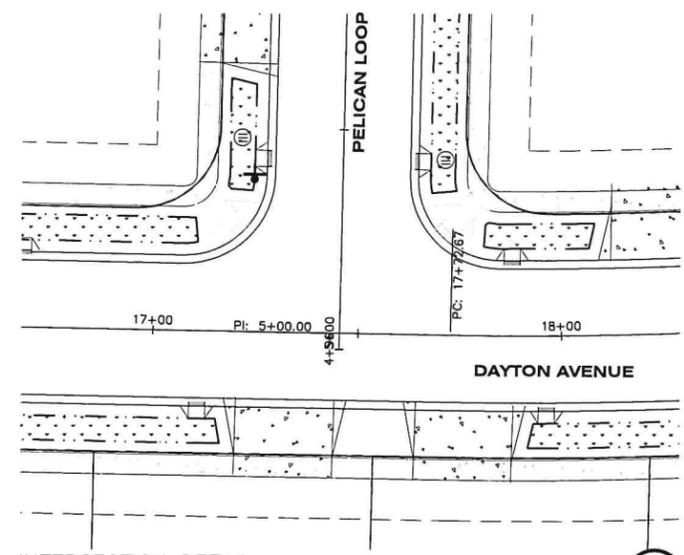
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ASTORIA LOOP & DAYTON AVENUE

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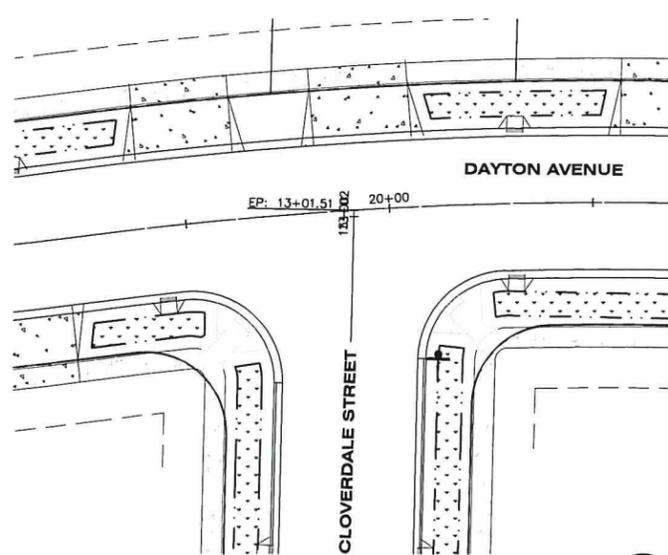
INTERSECTION DETAIL  
PELICAN LOOP & DAYTON AVENUE

4



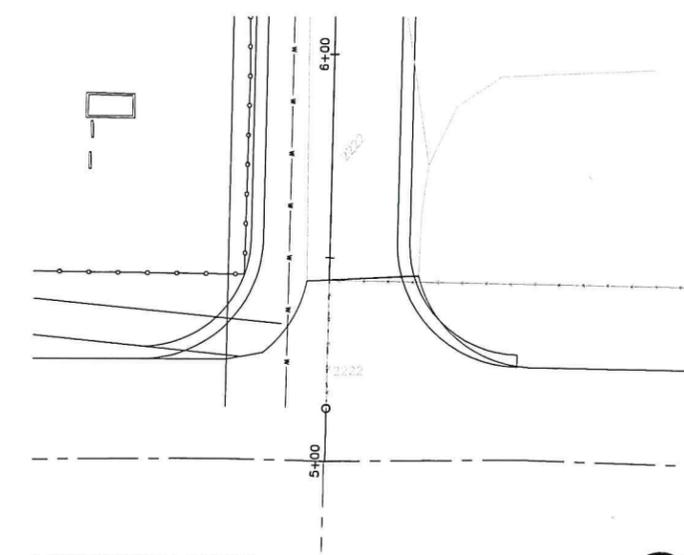
INTERSECTION DETAIL  
PELICAN LOOP & DAYTON AVENUE  
SCALE: 1"=20'

5



INTERSECTION DETAIL  
CLOVERDALE STREET & DAYTON AVENUE

6



INTERSECTION DETAIL  
MEYER ROAD & HWY 53

7

PROJECT: 19-2290 WESTWOOD PINES STREET WATER SERVICE PLAN DATE: 12/17/19

NAVD-88  
XXXX

NO.	DATE	BY	REVISIONS
A	1-23-19	RDK	ORIGINAL PREPARATION

SCALE:  
HORIZONTAL: N/A  
VERTICAL: N/A

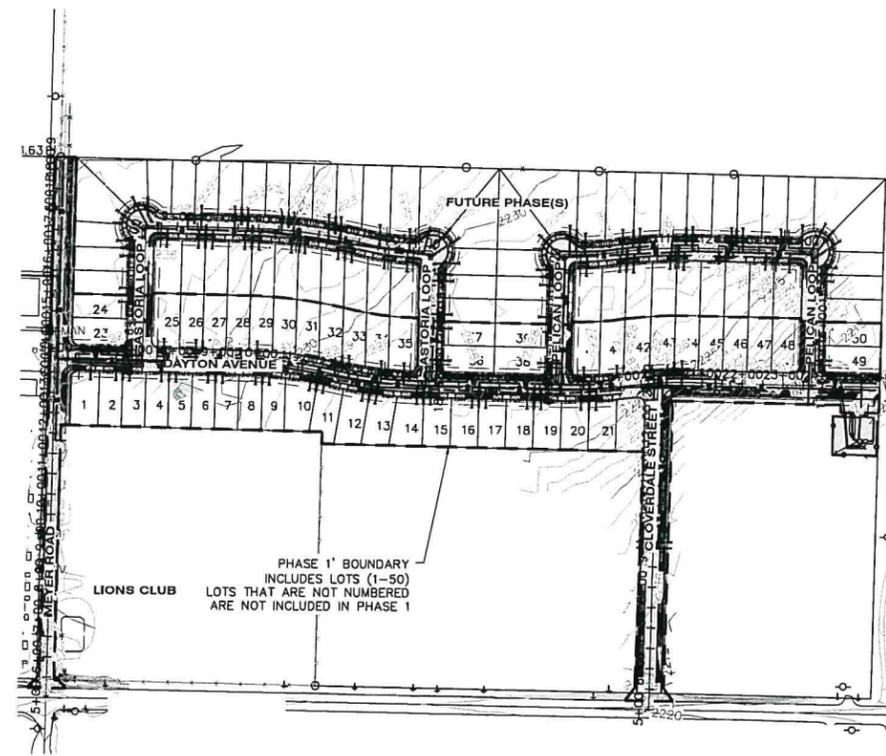
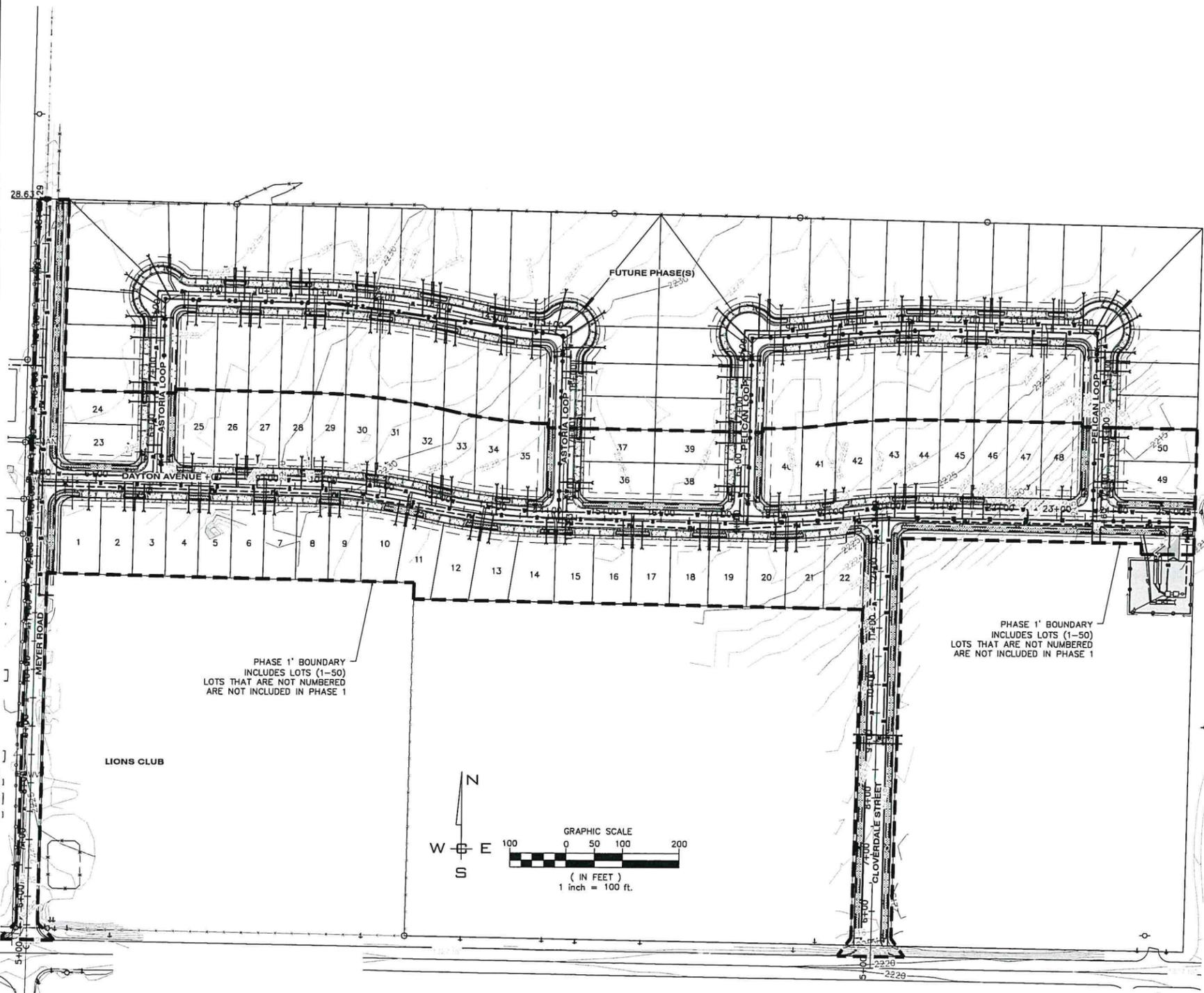
PROJ #: 19-2290  
DATE: 12/06/19  
DRAWN: CTR  
REVIEWED: RDK

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<input type="checkbox"/>	SURVEYING
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<input type="checkbox"/>	OTHER



**WESTWOOD PINES  
STREET DETAILS  
RATHDRUM, ID**

SHEET  
C3.5  
JOB NUMBER  
19-2290



GRADING PLAN WITH EXISTING CONTOURS  
SCALE: 1"=200'

GRADING PLAN  
SCALE: 1"=100'

NAVD-88  
XXXX

A	1-23-18	RDK	ORIGINAL PREPARATION
NO.	DATE	BY	REVISIONS

SCALE:  
HORIZONTAL:  
AS SHOWN  
VERTICAL:  
N/A

PROJ #: 19-2290  
DATE: 12/06/19  
DRAWN: CTR  
REVIEWED: RDK

- CIVIL
- STRUCTURAL
- SURVEYING
- TRAFFIC
- PLANNING
- LANDSCAPE
- OTHER

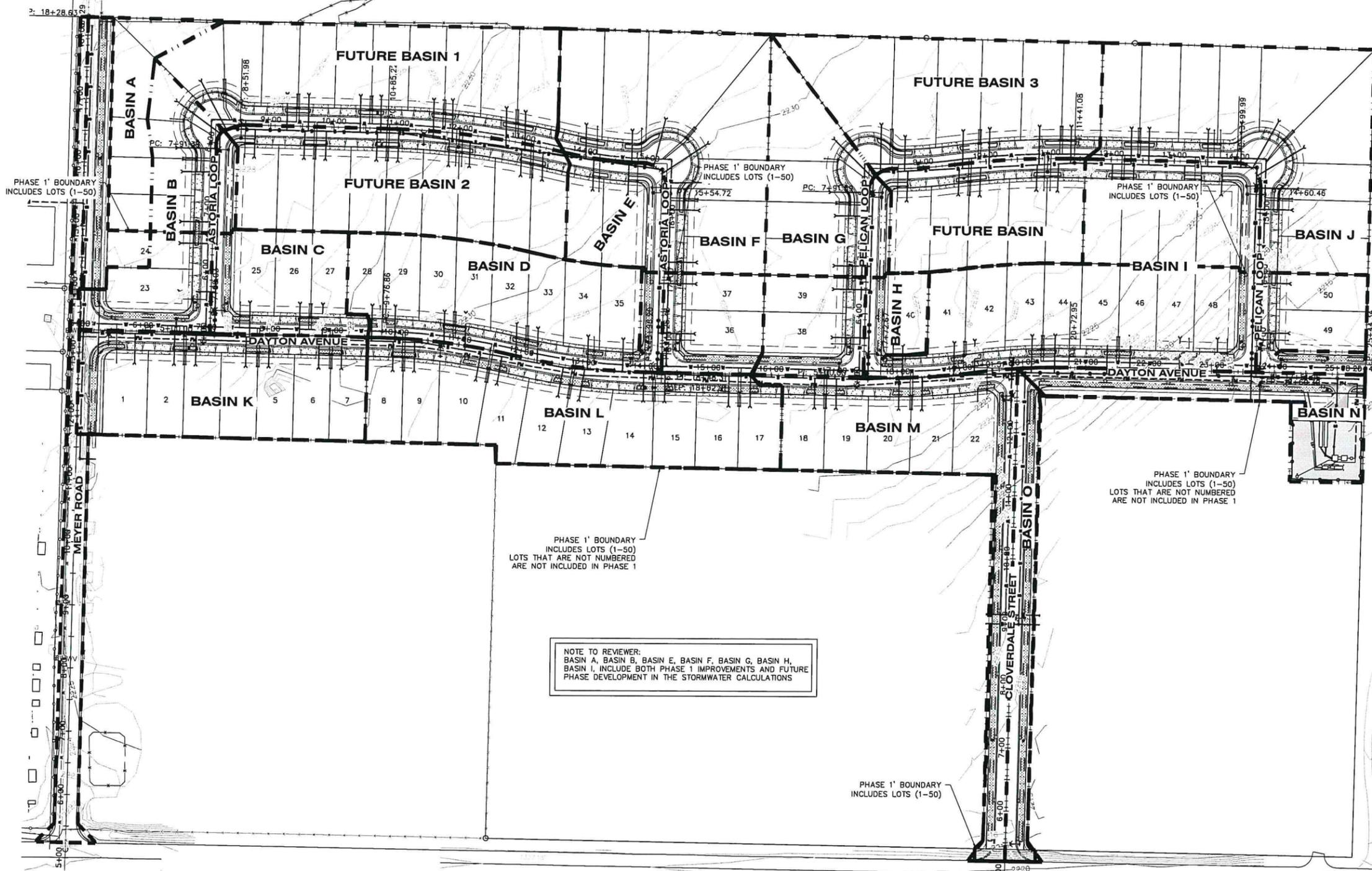


WESTWOOD PINES  
GRADING  
RATHDRUM, ID

SHEET  
C4.0  
JOB NUMBER  
19-2290

SW 1/4, SEC.29, T.52N., R.04E., B.M.

UNDERGROUND SERVICE ALERT  
ONE-CALL NUMBER  
**811**  
CALL TWO BUSINESS DAYS  
BEFORE YOU DIG



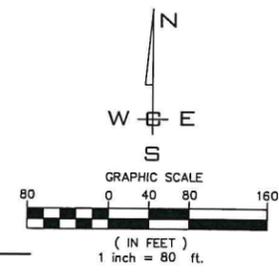
PHASE 1' BOUNDARY  
INCLUDES LOTS (1-50)  
LOTS THAT ARE NOT NUMBERED  
ARE NOT INCLUDED IN PHASE 1

PHASE 1' BOUNDARY  
INCLUDES LOTS (1-50)  
LOTS THAT ARE NOT NUMBERED  
ARE NOT INCLUDED IN PHASE 1

NOTE TO REVIEWER:  
BASIN A, BASIN B, BASIN E, BASIN F, BASIN G, BASIN H,  
BASIN I, INCLUDE BOTH PHASE 1 IMPROVEMENTS AND FUTURE  
PHASE DEVELOPMENT IN THE STORMWATER CALCULATIONS

PHASE 1' BOUNDARY  
INCLUDES LOTS (1-50)

**STORM DRAINAGE PLAN**  
SCALE: 1"=80'



BASIN TABLE			
BASIN	SWALE AREA REQUIRED (FT <sup>2</sup> )	SWALE AREA PROVIDED (FT <sup>2</sup> )	SWALE BOTTOM ELEVATION (FT)
A	448	697	VARIES
B	458	582	VARIES
C	733	2019	VARIES
D	810	1871	VARIES
E	749	1593	VARIES
F	411	1636	VARIES

BASIN TABLE			
BASIN	SWALE AREA REQUIRED (FT <sup>2</sup> )	SWALE AREA PROVIDED (FT <sup>2</sup> )	SWALE BOTTOM ELEVATION (FT)
C	448	697	VARIES
H	458	582	VARIES
I	733	2019	VARIES
J	810	1871	VARIES
K	749	1593	VARIES
L	411	1636	VARIES

BASIN TABLE			
BASIN	SWALE AREA REQUIRED (FT <sup>2</sup> )	SWALE AREA PROVIDED (FT <sup>2</sup> )	SWALE BOTTOM ELEVATION (FT)
M	448	697	VARIES
N	458	582	VARIES
O	733	2019	VARIES
FUTURE 1	810	1871	VARIES
FUTURE 2	749	1593	VARIES
FUTURE 3	411	1636	VARIES

**CONSTRUCTION NOTES**

- 1 PROVIDE AND INSTALL TYPE "A" DRYWELL PER CITY OF RATHDRUM STANDARD PLAN.
- 2 PROVIDE AND INSTALL TYPE "B" DRYWELL PER CITY OF RATHDRUM STANDARD PLAN.
- 3 CONSTRUCT ROADSIDE SWALE PER CITY OF RATHDRUM TYPICAL SWALE DESIGN STANDARD PLAN.

NAVD-88  
XXXX

SCALE:	
HORIZONTAL:	1" = 80'
VERTICAL:	N/A

PROJ #:	19-2290
DATE:	12/06/19
DRAWN:	CTR
REVIEWED:	RDk

<input checked="" type="checkbox"/> CIVIL
<input type="checkbox"/> STRUCTURAL
<input type="checkbox"/> SURVEYING
<input type="checkbox"/> TRAFFIC
<input type="checkbox"/> PLANNING
<input type="checkbox"/> LANDSCAPE
<input type="checkbox"/> OTHER

**WCE**  
WHIPPLE CONSULTING ENGINEERS  
21 SOUTH PINES ROAD  
SPOKANE VALLEY, WA 99208  
PH: 509-933-2817 FAX: 509-933-0227

**WESTWOOD PINES  
DRAINAGE PLAN  
RATHDRUM, ID**

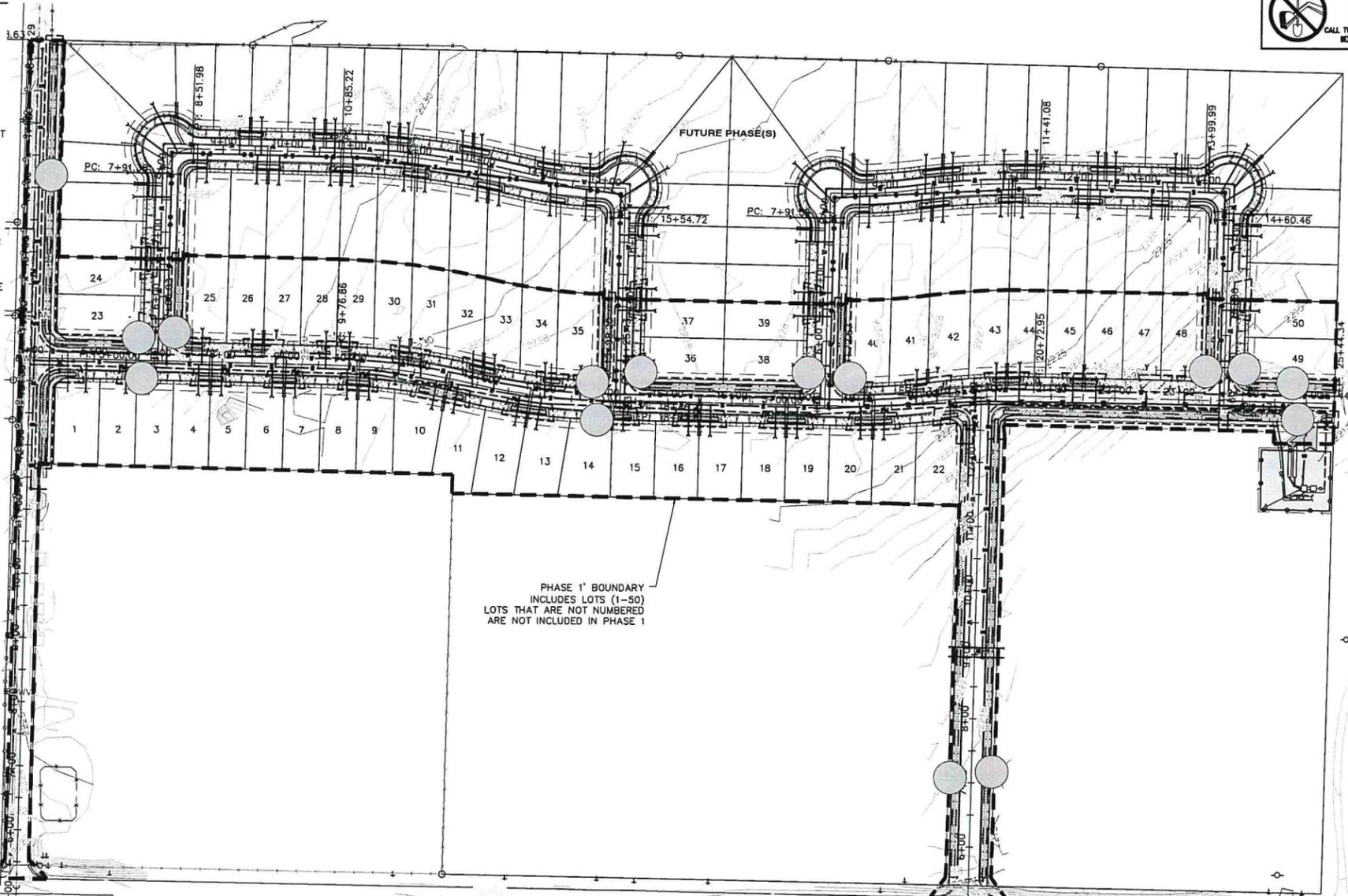
**SHEET  
C5.0**  
JOB NUMBER  
**19-2290**

PROJECT: WESTWOOD PINES DRAINAGE PLAN - RATHDRUM, ID. DATE: 12/06/19. DRAWN BY: CTR. CHECKED BY: RDk.



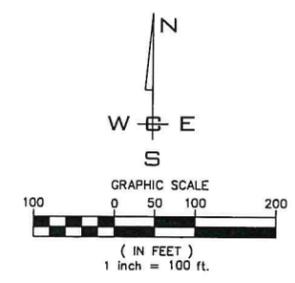
**GENERAL NOTES**

1. ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION OF SITE IMPROVEMENTS SHALL MEET OR EXCEED SITE WORK STANDARDS AND THE STANDARDS AND SPECIFICATIONS SET FORTH IN THE CITY OF RATHDRUM REGULATIONS AND APPLICABLE STATE AND FEDERAL REGULATIONS. WHERE THERE IS CONFLICT BETWEEN THESE PLANS AND THE SPECIFICATIONS, OR ANY APPLICABLE STANDARDS, THE HIGHER QUALITY STANDARD SHALL APPLY. ALL WORK WITHIN PUBLIC R.O.W. OR EASEMENTS SHALL BE INSPECTED AND APPROVED BY THE CITY OF RATHDRUM, INSPECTOR. INSPECTION SERVICES AND CONSTRUCTION CERTIFICATION TO BE PROVIDED BY DESIGNEE OF PROJECT SPONSOR/OWNER.
2. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES, AS SHOWN ON THESE PLANS, IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY PERTINENT LOCATIONS AND ELEVATIONS, ESPECIALLY AT THE CONNECTION POINTS AND AT POTENTIAL UTILITY CONFLICTS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM ALL APPLICABLE AGENCIES. THE CONTRACTOR SHALL NOTIFY THE CITY OF RATHDRUM INSPECTOR AT LEAST 48 HOURS PRIOR TO THE START OF ANY EARTH DISTURBING ACTIVITY OR CONSTRUCTION ON ANY AND ALL PUBLIC IMPROVEMENTS.
4. THE CONTRACTOR SHALL COORDINATE AND COOPERATE WITH THE CITY OF RATHDRUM AND ALL UTILITY COMPANIES WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION, TO ASSURE THAT THE WORK IS ACCOMPLISHED IN A TIMELY FASHION, AND WITH A MINIMUM DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL PARTIES AFFECTED BY ANY DISRUPTION OF ANY UTILITY SERVICE.
5. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE APPROVED PLANS, ONE (1) COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND ONE (1) COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED FOR THE JOB ON-SITE AT ALL TIMES.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF SAFETY INCLUDING, BUT NOT LIMITED TO: EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.
7. IF, DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED BY THE CONTRACTOR, HIS SUBCONTRACTORS, OR OTHER AFFECTED PARTIES WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY.
8. ALL REFERENCES TO ANY PUBLISHED STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARD, UNLESS SPECIFICALLY STATED OTHERWISE.
9. FOR WORK AFFECTING PUBLIC ROADWAYS OR IF REQUIRED BY THE CITY OF RATHDRUM, THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL AND PHASING PLAN IN ACCORDANCE WITH M.U.T.C.D. FOR APPROVAL PRIOR TO ANY CONSTRUCTION ACTIVITIES WITHIN OR AFFECTING THE RIGHT-OF-WAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY AND ALL TRAFFIC CONTROL DEVICES AS MAY BE REQUIRED BY SAID PLANS. PRIOR TO INSTALLATION, A PRECONSTRUCTION CONFERENCE SHALL BE HELD WITH THE CITY OF RATHDRUM.
10. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL LABOR AND MATERIALS NECESSARY FOR THE COMPLETION OF THE INTENDED IMPROVEMENTS SHOWN ON THESE DRAWINGS OR DESIGNATED TO BE PROVIDED, INSTALLED, CONSTRUCTED, REMOVED OR RELOCATED UNLESS SPECIFICALLY NOTED OTHERWISE.
11. PER AGENCY STANDARDS THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION DEBRIS AND DIRT TRACKED FROM THE SITE.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING RECORD INFORMATION ON A SET OF RECORD DRAWINGS KEPT AT THE CONSTRUCTION SITE AND AVAILABLE TO THE CITY OF RATHDRUM INSPECTOR AT ALL TIMES.
13. DIMENSIONS FOR LAYOUT AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWING. FOR ADDITIONAL INFORMATION CONTACT THE ENGINEER FOR CLARIFICATION AND NOTE ON THE RECORD DRAWINGS.
14. ALL EROSION AND SEDIMENT CONTROL (E.S.C.) MEASURES SHALL BE INSTALLED AT THE LIMITS OF CONSTRUCTION PRIOR TO GROUND DISTURBING ACTIVITY. ALL E.S.C. MEASURES SHALL BE MAINTAINED IN GOOD REPAIR BY THE CONTRACTOR UNTIL SUCH TIME AS THE ENTIRE DISTURBED AREAS ARE STABILIZED WITH HARD SURFACE OR LANDSCAPING.
15. THE CONTRACTOR SHALL SEQUENCE INSTALLATION OF UTILITIES IN SUCH A MANNER AS TO MINIMIZE POTENTIAL UTILITY CONFLICTS. IN GENERAL, STORM SEWER AND SANITARY SEWER SHOULD BE CONSTRUCTED PRIOR TO INSTALLATION OF WATER LINES AND DRY UTILITIES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE ALL UTILITY RELOCATIONS CONSISTENT WITH THE CONTRACTORS SCHEDULE FOR THIS PROJECT, WHETHER SHOWN OR NOT SHOWN, AS IT RELATES TO THE CONSTRUCTION ACTIVITIES CONTEMPLATED IN THESE PLANS.
16. ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY IS SUBJECT TO THE JURISDICTION OF THE CITY OF RATHDRUM ENGINEERING DEPARTMENT STANDARD DETAILS AND SPECIFICATIONS.
17. ALL CONSTRUCTION OPERATIONS, INCLUDING THE WARMING UP, REPAIR, ARRIVAL, DEPARTURE OR RUNNING OF TRUCKS, EARTH MOVING EQUIPMENT, CONSTRUCTION EQUIPMENT AND ANY OTHER ASSOCIATED EQUIPMENT SHALL GENERALLY BE LIMITED TO THE TIME PERIOD APPROVED BY THE CITY OF RATHDRUM.
18. BASED ON REQUIREMENTS FROM THE CITY OF RATHDRUM, THE ENGINEER OR HIS DESIGNEE SHALL PERFORM MATERIALS TESTING AND QUALITY CONTROL ON THE PROJECT AND SHALL SUBMIT COPIES OF DAILY REPORTS, TEST REPORTS, PROJECT CERTIFICATION AND RECORD DRAWINGS TO THE CITY OF RATHDRUM, ENGINEER.
19. NO REVISIONS SHALL BE MADE TO THESE PLANS WITHOUT APPROVAL OF THE CITY OF RATHDRUM ENGINEERS AND NOTIFICATION OF THE ENGINEER OF RECORD.
20. SPECIAL CARE SHALL BE TAKEN TO AVOID TRACK OUT ONTO PAVED STREETS. ANY PAVED STREET USED FOR ACCESS SHALL HAVE A ROCK CONSTRUCTION ENTRANCE. ANY STREET NOT USED FOR SITE ACCESS SHALL BE BLOCKED TO PREVENT EGRESS AND TRACK OUT. SHOULD TRACK OUT OCCUR, THE CONTRACTOR IS RESPONSIBLE FOR SWEEPING AS DIRECTED BY THE CITY INSPECTOR OR THE PROJECT ENGINEER.
21. THE CONTRACTOR IS CAUTIONED THAT IT IS THE UNDERSTANDING OF THE OWNER AND THE ENGINEER THAT SHOULD A CONFLICT OR DISCREPANCY IN THESE PLANS, SPECIFICATIONS, GENERAL NOTES OR PLANS E.T.A.L. DETERMINED TO BE PART OF THE OVERALL PROJECT, INCLUDING BUT NOT LIMITED TO THE ARCHITECTURAL PLANS, MECHANICAL PLANS, ELECTRICAL PLANS, LANDSCAPE PLANS, GENERAL SPECIAL PROVISIONS, ETC., THAT WITHOUT WRITTEN CLARIFICATION FROM THE ENGINEER, OWNER OR OTHER PROFESSIONAL, DURING THE BIDDING PROCESS, THAT IN ALL INSTANCES THE CONTRACTOR WILL BE REQUIRED TO BID THE HIGHER STANDARD. FAILURE TO DO SO MAY RESULT IN THE HIGHER STANDARD BEING REQUIRED BY THE OWNER, ENGINEER OR OTHER PROFESSIONAL WITH NO CHANGE IN VALUE TO THE CONTRACT VIA CHANGE ORDER OR OTHER MECHANISM.
22. CONSTRUCTION OF EVERY DRYWELL, INCLUDING FABRIC AND DRAINROCK, SHALL BE OBSERVED BY THE ON-SITE INSPECTOR TO CONFIRM THAT IT MEETS THE DESIGN DETAILS AND SPECIFICATIONS. DRYWELLS NOT OBSERVED SHALL HAVE THEIR PERFORMANCE VERIFIED BY A FULL-SCALE DRYWELL TEST.



**LEGEND**

- STORM DRAINAGE POND - NO CONCRETE TRUCK WASHOUT AREA.
- SILT FENCE - AS NEEDED
- ROCK CONSTRUCTION ENTRY. GENERAL NOTE 6.
- SEDIMENT FILTER - GENERAL NOTE 4
- CONCRETE WASHOUT AREA- MAY ONLY TAKE PLACE ON UNCOMPACTED SUBGRADE AFTER ASPHALT REMOVAL, AND IN A NON-LANDSCAPED AREA. OTHERWISE ANY CONCRETE WASHOUT MUST BE OFF SITE. GENERAL NOTE 7
- LIMITS OF DISTURBANCE



PROJECT NO. 19-2290 WESTWOOD PINES EROSION CONTROL PLAN DATE 12/06/19  
 DRAWN BY: CTR  
 CHECKED BY: RDK  
 DATE: 12/06/19

NAVD-88  
XXXX

SCALE:	
HORIZONTAL:	1" = 100'
VERTICAL:	N/A

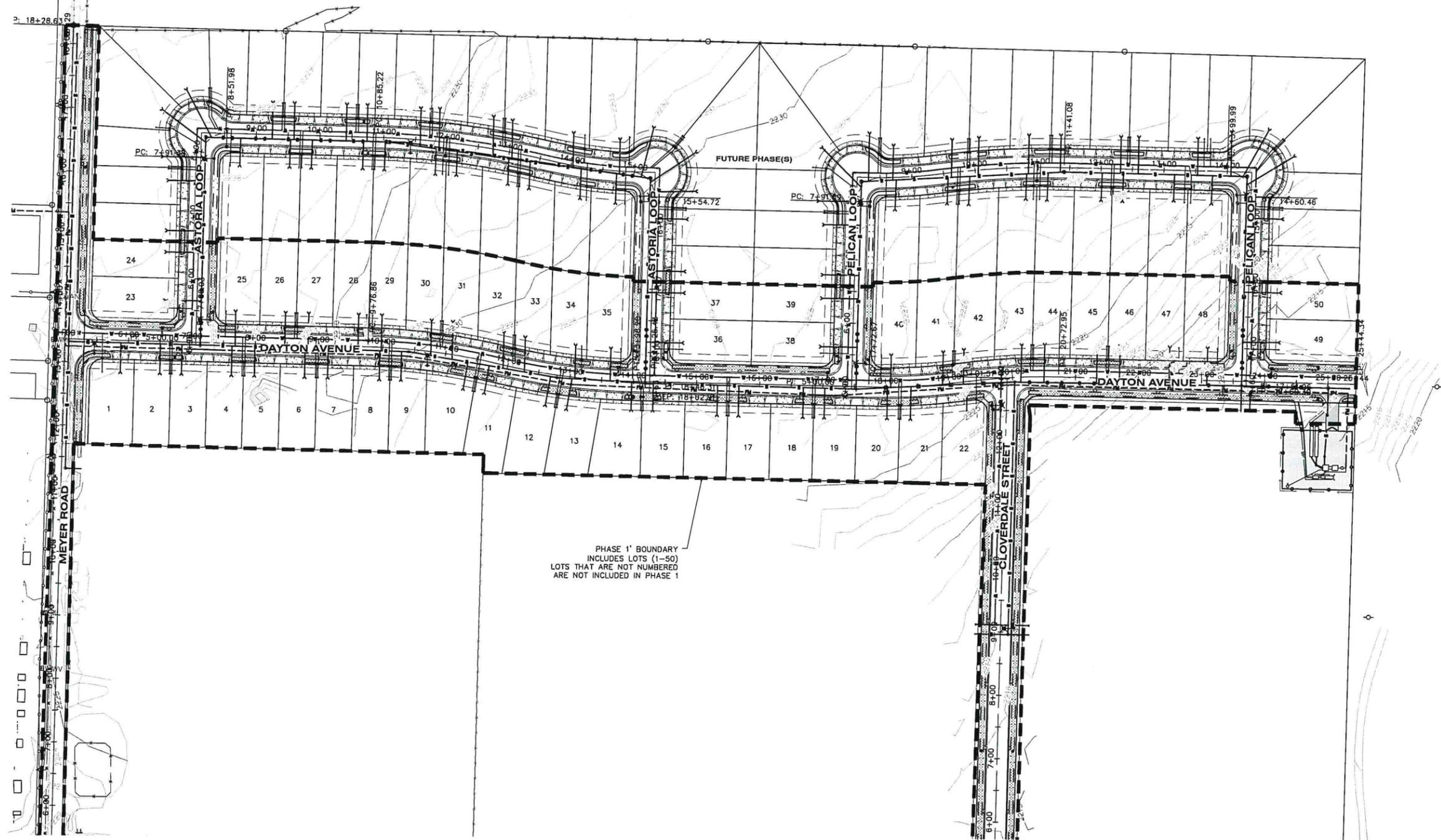
NO.	DATE	BY	REVISIONS
A	1-23-19	RDK	ORIGINAL PREPARATION

PROJ #:	19-2290
DATE:	12/06/19
DRAWN:	CTR
REVIEWED:	RDK

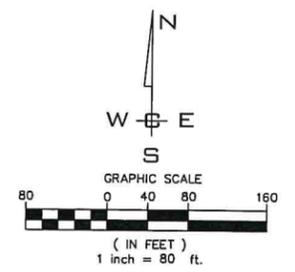
**WCE**  
 WHIPPLE CONSULTING ENGINEERS  
 21 SOUTH PINES ROAD  
 SPOKANE VALLEY, WA 99208  
 PH: 509-893-0817 FAX: 509-926-0227

**WESTWOOD PINES  
 EROSION CONTROL PLAN  
 RATHDRUM, ID**

**SHEET  
 C5.1**  
 JOB NUMBER  
**19-2290**



PHASE 1' BOUNDARY  
INCLUDES LOTS (1-50)  
LOTS THAT ARE NOT NUMBERED  
ARE NOT INCLUDED IN PHASE 1



**UTILITY OVERVIEW PLAN**  
SCALE: 1"=80'

- NOTE:
1. DEPTH OF SEWER SERVICES ARE LABELED TO THE NEAREST HALF FOOT. AT BACK OF WALK.
  2. UTILITY SERVICES ARE TO BE SLEEVED WHERE NECESSARY.

NAVD-88  
XXXX

A		1-23-19 RDK		ORIGINAL PREPARATION	
NO.	DATE	BY			REVISIONS

**SCALE:**  
HORIZONTAL:  
1" = 80'  
VERTICAL:  
N/A

PROJ #: 19-2290  
DATE: 12/06/19  
DRAWN: CTR  
REVIEWED: RDK

- CIVIL
- STRUCTURAL
- SURVEYING
- TRAFFIC
- PLANNING
- LANDSCAPE
- OTHER



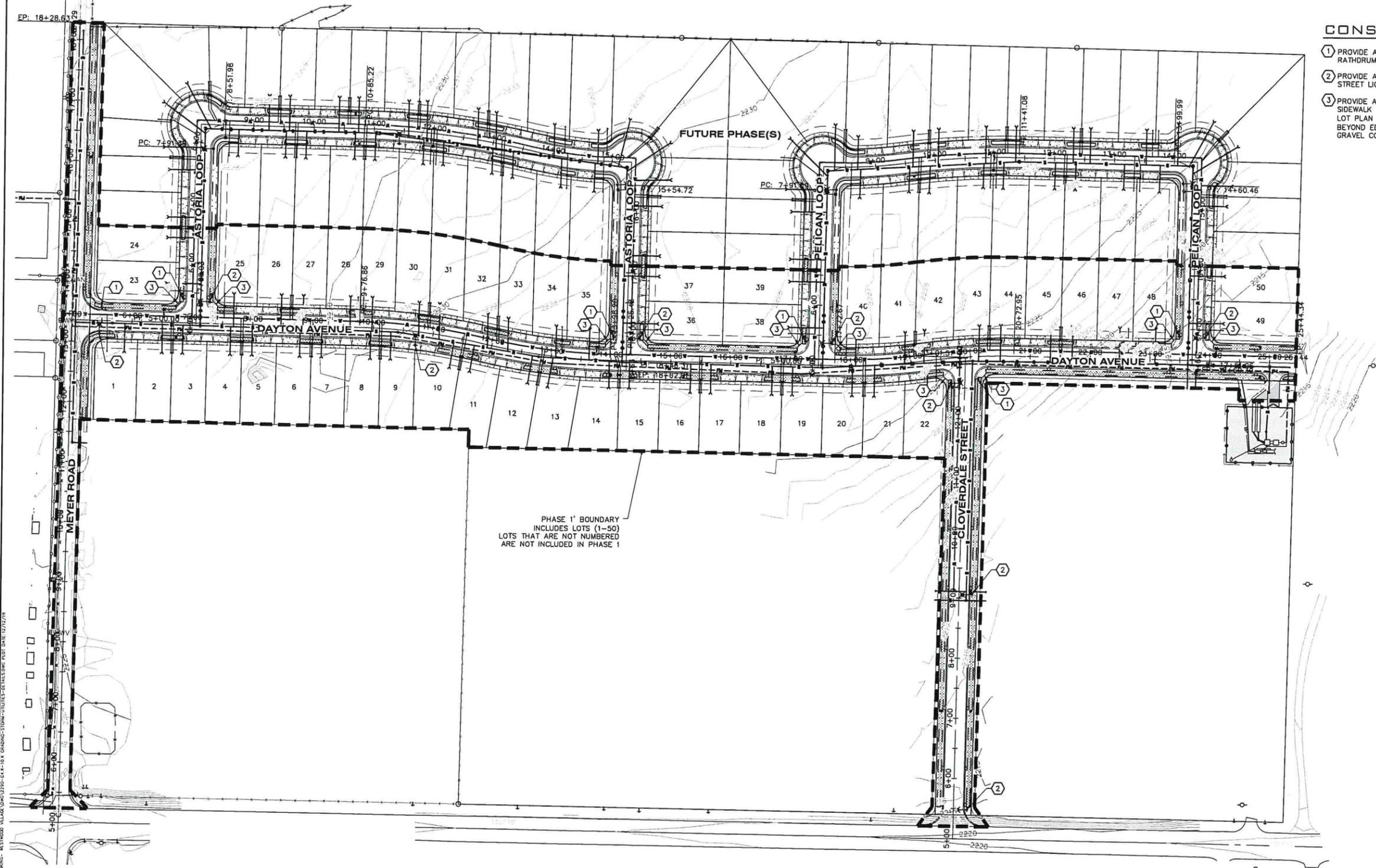
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UTILITY OVERVIEW  
RATHDRUM, ID**

**SHEET  
C7.0**  
JOB NUMBER  
**19-2290**

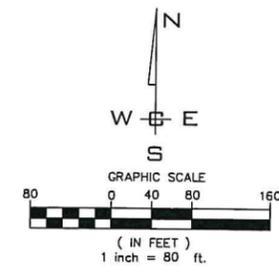


**CONSTRUCTION NOTES**

- ① PROVIDE AND INSTALL STREET/STOP (R1-1) PER CITY OF RATHDRUM DETAIL.
- ② PROVIDE AND INSTALL 100W LED TOWN AND COUNTRY STREET LIGHT.
- ③ PROVIDE AND INSTALL 2" PVC IRRIGATION CONDUIT UNDER SIDEWALK PER THE CITY OF RATHDRUM TYPICAL LANDSCAPE LOT PLAN ON SHEET C10.0, EXTEND A MINIMUM OF 6" BEYOND EDGE OF CONCRETE FORMS AND PROVIDE 4" OF GRAVEL COVER OVER TOP.



PHASE 1' BOUNDARY  
INCLUDES LOTS (1-50)  
LOTS THAT ARE NOT NUMBERED  
ARE NOT INCLUDED IN PHASE 1



**SLEEVING, LIGHTING, & SIGNAGE PLAN**  
SCALE: 1"=80'

NAVD-88  
XXXX

NO.	DATE	BY	ORIGINAL PREPARATION	REVISIONS
A	1-23-18	RDK	ORIGINAL PREPARATION	

**SCALE:**  
HORIZONTAL:  
1" = 80'  
VERTICAL:  
N/A

PROJ #: 19-2290  
DATE: 12/06/19  
DRAWN: CTR  
REVIEWED: RDK

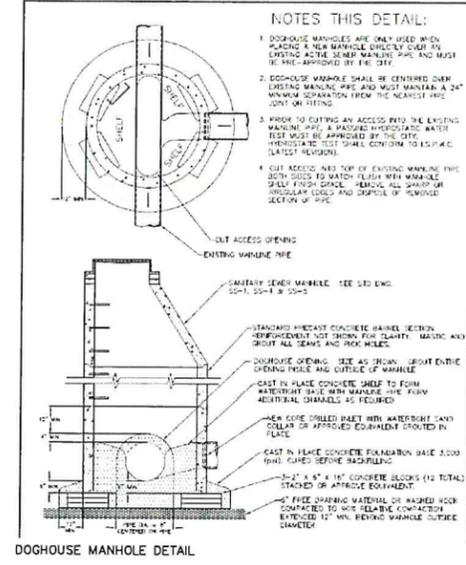
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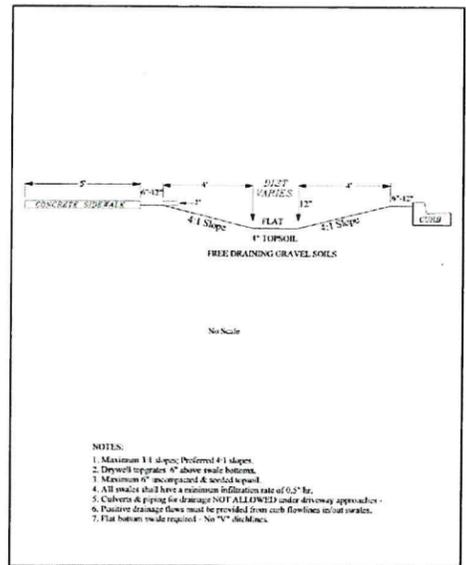
**WESTWOOD PINES**  
**SLEEVING, LIGHTING & SIGNAGE PLAN**  
**RATHDRUM, ID**

**SHEET**  
**C9.0**  
**JOB NUMBER**  
**19-2290**

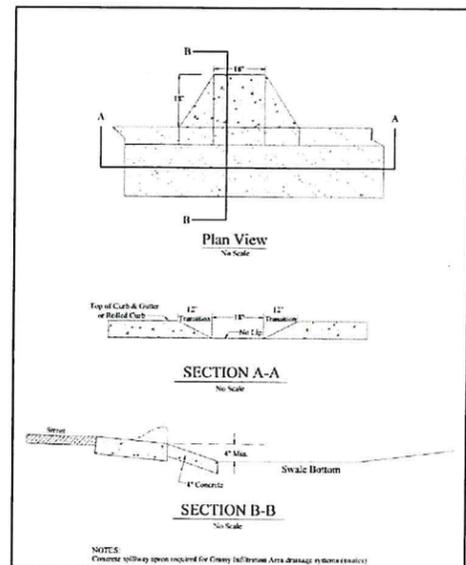
**UNDERGROUND SERVICE ALERT**  
**ONE-CALL NUMBER**  
**811**  
 CALL TWO BUSINESS DAYS  
 BEFORE YOU DIG



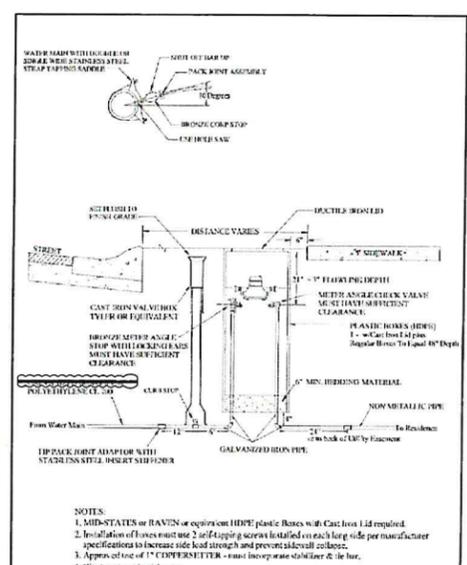
**DOGHOUSE MANHOLE DETAIL**



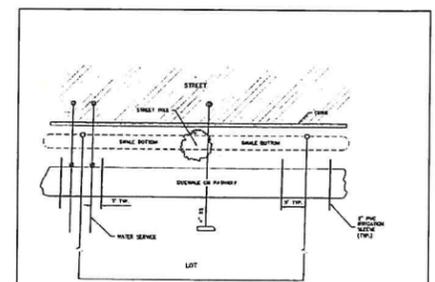
**TYPICAL SWALE DESIGN**



**CURB DRAIN INLET**

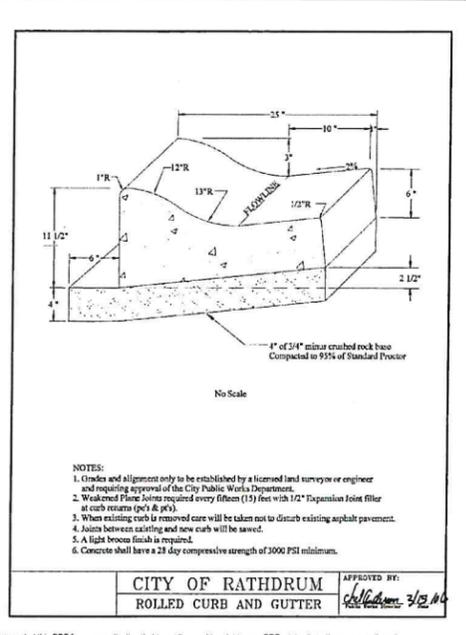


**WATER SERVICE CONNECTION**

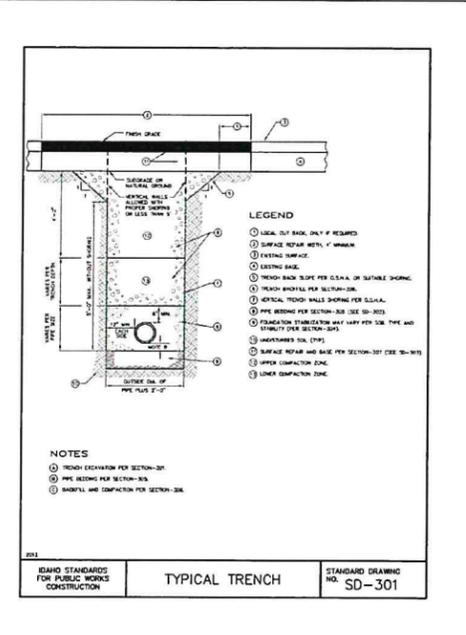


**TYPICAL LOT LANDSCAPE PLAN**

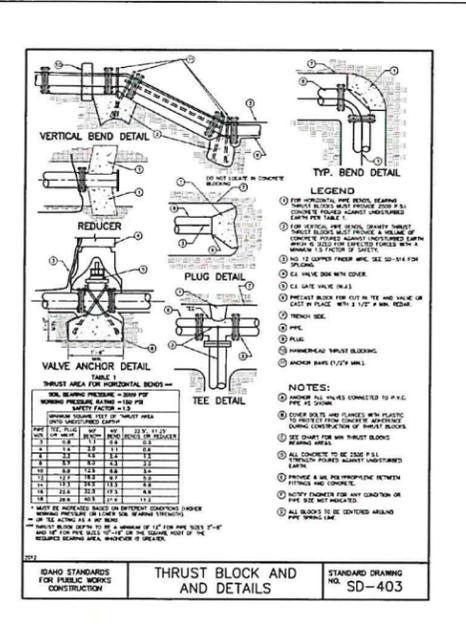
CONTRACTOR IS RESPONSIBLE FOR HAVING A COMPLETE SET OF THE CITY OF RATHDRUM'S STANDARDS ONSITE AT ALL TIMES.



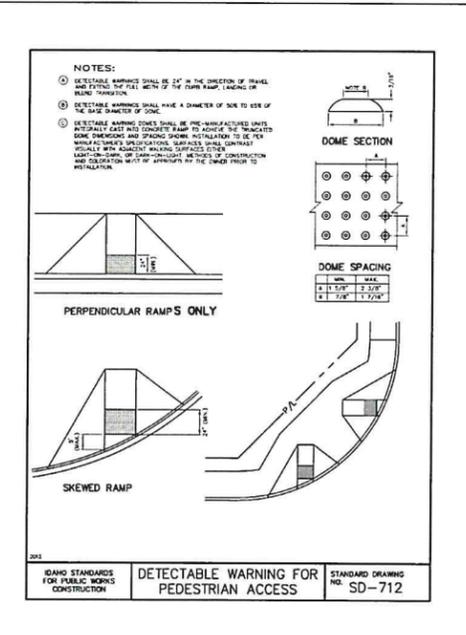
**ROLLED CURB AND GUTTER**



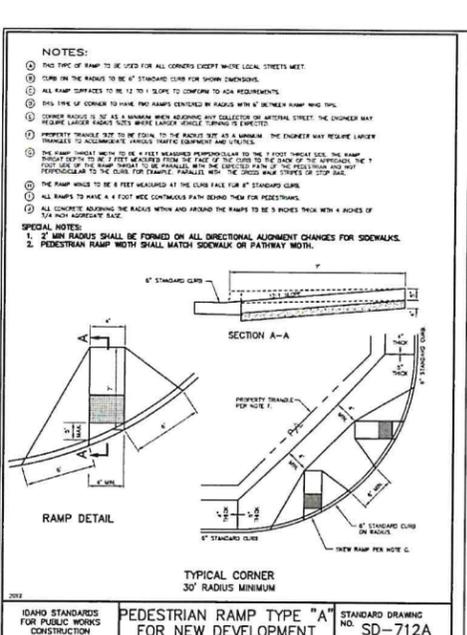
**TYPICAL TRENCH**



**THRUST BLOCK AND DETAILS**



**DETECTABLE WARNING FOR PEDESTRIAN ACCESS**



**PEDESTRIAN RAMP TYPE "A" FOR NEW DEVELOPMENT**

SCALE:	
HORIZONTAL:	N/A
VERTICAL:	N/A

PROJ #:	19-2290
DATE:	12/06/19
DRAWN:	CTR
REVIEWED:	RDK



**WESTWOOD PINES**  
**CONSTRUCTION DETAILS**  
**RATHDRUM, ID**

**SHEET**  
**C10.0**  
**JOB NUMBER**  
**19-2290**

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CLIFFORD T. HAYES RP I 2301043000  
KOOTENAI CO. RECORDER Page 1 of 8  
AAA Date 02/01/2011 Time 09:05:15  
REC-REQ OF RATHDRUM CITY  
RECORDING FEE: 0.00  
2301043000 XX 7

VIKING ANNEXATION AGREEMENT

THIS AGREEMENT is made effective this 2<sup>nd</sup> day of December, 2010, by and between the CITY OF RATHDRUM, a municipal corporation organized pursuant to the laws of the State of Idaho, hereinafter termed the "City," and VIKING CONSTRUCTION, INC, an Idaho corporation, hereinafter termed "Viking".

RECITALS

- A. Viking is the owner of record of real property located along and near the boundary of Rathdrum in Kootenai County, Idaho, which property is described and depicted in Exhibits "A-1" and "A-2" (herein "the Property") attached hereto and incorporated herein by reference.
- B. The Property is not presently located within the corporate limits of any municipality, but is located adjacent to and contiguous with the corporate city limits of the City and may be annexed to the City as provided in Idaho Code § 50-222.
- C. Viking has filed with the City a complete and proper Petition for Annexation and is the owner of land requesting that the Property be annexed.
- D. Pursuant to the applicable provisions of the Idaho Code, the Viking's Petition for Annexation was submitted to the Planning and Zoning Commission for the City and a public hearing was held pursuant to notice, as provided by statute. Following public hearing and comment, the Planning and Zoning Commission did resolve by vote of the Commissioners to recommend that the City Council approve annexation of the Property subject to conditions.
- E. Pursuant to the applicable provisions of the Idaho Code, the recommendation of the Planning and Zoning Commission and the Viking's Petition for Annexation was duly submitted to the City Council, and a public hearing was held pursuant to notice as provided by statute. Following public hearing and comment, the City Council did determine by vote of the Council members to accept the recommendation of the Planning and Zoning Commission and agreed to annex the Property subject to the terms and conditions of an Annexation Agreement to be entered into by Parties.
- F. All other and further notices, publications, procedures, public hearings, and other matters attendant to the consideration and approval of this Agreement and the annexation of the Property have been given, made, held and performed by the City as required by the applicable provisions of the Idaho Code and all applicable ordinances, regulations, and procedures of the City.

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G. The City Council having duly considered the Viking's Petition for Annexation; having considered the recommendation of the Planning and Zoning Commission; having considered public comment; having considered the terms and conditions of this Agreement; have authorized the Mayor to execute, and the City Clerk to attest, this Agreement on behalf of the City.

H. The City acknowledges that Viking's proposed use of the Property and the annexation of the Property by the City, subject to development of viable utility and transportation facilities wherein Viking's participation will be necessary, should be compatible with and should further the planning objectives of the City; will extend the corporate limits and jurisdiction of the City; and will permit orderly growth, planning, and development of the City.

I. The Parties desire to enter into this Agreement pursuant to the applicable provisions of the Idaho Code and in accordance with the terms and conditions set forth herein.

NOW THEREFORE, IN CONSIDERATION of the covenants and conditions set forth herein, the parties agree as follows:

1. Recitals. The Parties acknowledge that the statements and representations contained in the above Recitals are true and accurate and incorporate those Recitals into this Agreement as if fully set forth in this Article 1.

2. Purpose. Viking enters into this Agreement to obtain annexation of the Property, while City seeks to obtain partial mitigation of the effects of annexation of the Property. Viking acknowledges that City has no duty to annex the Property and that the promises of Viking constitute an inducement for City to do so. The term "Viking" is deemed to include any successor in interest to the Property.

3. Annexation of the Property. Following the execution of this Agreement by the Parties, the City will proceed, subject to the terms and conditions set forth in this Agreement, to do all things necessary and appropriate to cause the Property to be annexed by lawful ordinance into the City.

3.1 Effective Date of Annexation. Upon the proper execution and recordation of this Agreement, the City will prepare for passage an annexation ordinance annexing Viking's property. Following passage of the annexation ordinance, the City Clerk shall cause the annexation ordinance to be duly published in the City's official newspaper. The parties agree that final annexation shall occur on the date of publication of the annexation ordinance.

3.2 Filing of Certified Copy of Annexation Ordinance. As required by law, the City Clerk shall file a certified copy of the annexation ordinance with the Kootenai County Auditor, the Kootenai County Treasurer, the Kootenai County Assessor, and the Idaho State Tax Commission to comply with the applicable provisions of the Idaho Code.

3.3 Recordation of Agreement. The City Clerk shall thereafter record this Agreement with the Kootenai County Recorder's Office. The City Clerk shall provide a copy of the recorded agreement to Viking.

4. Water Service.

4.1 Agreement to Use. Viking agrees to use the City's water supply system to serve the future development of the Property and agrees to pay all required hook-up fees and uniform capitalization charges as established by ordinance. Viking agrees it shall be solely responsible for carrying out any system extension to connect proposed development to the existing City system in the size and location reasonably required by the City. Viking also agrees to dedicate to City at the time of development that portion of its water right that is appurtenant to the annexed Property in order that such rights might be combined with the City's existing water rights in order that water can be supplied to the uses on the subject property in future years. Such water rights shall be dedicated to the City at the time of annexation if not currently in use or anticipated to be used during the next growing season.

4.2 Water System Improvements. Viking agrees to deed to the City a well lot and necessary easements and rights-of-way, if found appropriate by the City, at the time of development of the property in order to enhance the City water supply, which will be a benefit the Viking property.

4.3 No Warranty of Water System Capacity. The City represents that it owns, operates, and maintains a potable water supply and distribution system within its borders, which water system and mains have at the present time projected sufficient capacity to accommodate the anticipated potable water and fire protection needs of the Property. Such available capacity may, however, be consumed by supplying developments within the City that precede development of the lands addressed by this annexation agreement. City does not warrant that it will have sufficient water system capacity to supply the needs of Viking when Viking desires to proceed with development.

5. Sanitary Sewer Service.

5.1 Agreement to Use. Viking agrees to use the City's sanitary sewer system to serve the future residential development of the Property and agrees to pay all required hook-up fees, including any connection fee to assist in providing the necessary capacity to its existing system.

5.2 Sanitary Sewer Improvements. Viking agrees that any interim sewer plan proposed by Viking must be reviewed and approved by the City engineer and subsequently by the City council as an amendment to the City's Sewer Master Plan before a provisional sewer arrangement will be allowed. Any provisional sewer arrangement will also need to assess how ultimate sewer plans will be funded and implements.

5.3. No Warranty of Capacity. The City represents that it owns, operates, and maintains a sanitary sewer collection system, which sewer system components and mains have, at the present time, sufficient capacity to accommodate the anticipated sanitary sewer needs of the Property. Such current condition is not warranted to remain forever. Further, treatment for wastewater from Rathdrum is provided pursuant to contract with the city of Post Falls, and Rathdrum cannot provide assurance that the Post Falls treatment facility will always have available capacity for development. Viking agrees it shall be solely responsible for carrying out any system extension to connect proposed development to the existing City system in the size and location reasonably required by the City.

6. Codes and Ordinances.

6.1 Applicable Standards. All codes, ordinances, rules, and regulations of the City in effect as of the date of proposed development shall govern future development of the Property.

6.2 Construct to City Standards. Viking agrees that all improvements required by this Agreement or any future Development Agreement shall be constructed to meet City standards subject to Article 6.1, above and to meet the standards of any other public agency providing service to the Property. If Viking fails to comply with applicable laws in the course of constructing improvements, public or otherwise, on the lands subject to this Agreement, or fails to comply with the provisions of this Agreement, Viking consents to suspension of issuance of building permits or denial of certificates of occupancy until such compliance is attained.

7. Dedication of Right-of-Way, Easements, Access and Public Transit.

7.1 Viking agrees that by right-of-way deed or easement of equal date with this Agreement, unless specifically provided otherwise, Viking will grant the rights-of-way and easements set forth herein.

7.1.1 Viking will grant right-of-way to provide a total of 40 feet of right-of-way east from the centerline of Meyer Road adjacent to the subject property.

7.1.2 Viking agrees to be responsible for the construction of the right-of-way improvements along Meyer Road from the northerly extent of the subject property to Highway 53 at the time of development of any part of the subject property zoned as Residential-R2S. In the event the development of the property zoned as Residential-R2S is proposed to be completed in phases, the Meyer Road improvements will be completed as part of the first phase. Said improvements shall be constructed at the same time to avoid discontinuous development of the improvements.

7.2 Any development proposal submitted by Viking must include a limitation of vehicular access from Highway 53 to no more than two points of access to service the entire property. Such access must meet the standards, goals and policies of the Transportation Plans and as approved by both the Idaho Transportation Department and the City.

7.3 At the time of development of the property, Viking shall provide within the development bus stop areas to assure future accessibility by public transit and school buses as required by the city when development plans are reviewed. Such areas shall be designated to provide safe and convenient staging areas for riders so as not to conflict with vehicular and pedestrian traffic and shall be at locations approved by the City.

8. Consideration. Viking agrees to provide specific consideration to the City in the amounts and at the times set forth herein. The sums specified are deemed by the Parties to be a reasonable consideration for City benefits to Viking's use or development of its lands annexed hereby, including, but not limited to: public safety, street services, police equipment, community and traffic planning and fire protection equipment/facilities. The following consideration may be used in any manner that the City, in its sole discretion decides.

8.1 Annexation Fee: Viking agrees to pay annexation fees in amounts as set by resolution of City council when the property is developed through site improvement or residential platting. The parties agree that the sum is to be paid as an annexation fee, as distinguished from capital facilities fee which might otherwise be an obligation associated with development of the Property.

9. Covenants to Run with the Land. The covenants herein to be performed by Viking, including annexation fees, shall be binding upon Viking and Viking's heirs, assigns, and successors in interest, and shall be deemed to be covenants running with the land.

10. Severability. Should any provision of this Agreement be declared invalid by a court of competent jurisdiction, the remaining provisions shall continue in full force and effect and be interpreted to effectuate the purposes of the entire Agreement to the greatest extent possible.

11. Merger. All promises and prior negotiations of the parties merge into this Agreement.

12. Amendment. This Agreement shall only be amended in writing and signed by both parties. The Parties agree that this Agreement shall not be amended by a change in law. The parties agree that this Agreement is not intended to replace any other requirement of City Code and that its execution shall not constitute a waiver of requirements established by City ordinance or other applicable provisions of law.

13. Remedies. Should either party require the services of legal counsel to enforce the terms of this Agreement, the prevailing party shall be entitled to its reasonable attorney's fees and related costs. This agreement shall be governed by Idaho law and resolution of any disputes

shall take place in courts of the state of Idaho, with venue in Kootenai County. The parties agree to meet and confer with one another prior to initiating legal action. Either party may comply with this provision by providing at least seven (7) days' written notice of a face-to-face meeting to be conducted at a Rathdrum location during regular business hours, even if the opposite party fails to appear or participate.

IN WITNESS WHEREOF, the City of Rathdrum has caused this Agreement to be executed by its Mayor and City Clerk, and Viking has executed this Agreement to be effective the day and year first above written.

CITY OF RATHDRUM

VIKING CONSTRUCTION, INC.

By: [Signature]  
Vic Holmes, Mayor  
Date: 12-29-10

By: [Signature]  
Date: 12-22-2010

Attest:

[Signature]  
Melissa Taylor, City Clerk

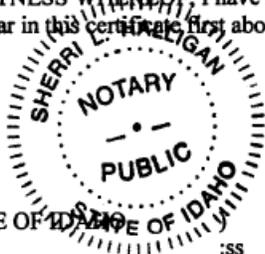


CITY ACKNOWLEDGMENT

STATE OF IDAHO )  
 ) ss.  
County of Kootenai )

On this 21 day of Dec, 20 10 before me, a Notary Public, personally appeared Vic Holmes and Melissa Taylor, known to me to be the Mayor and City Clerk, respectively, of the City of Rathdrum that executed the foregoing instrument and acknowledged to me that said City of Rathdrum executed the same.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my Notarial Seal the day and year in this certificate first above written.



[Signature]  
Notary Public for Idaho  
Residing at: 1801 Valle, ID  
My Commission expires: 7/17/13

STATE OF IDAHO )  
 ) ss.  
County of Kootenai )

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On this 22 day of December, 2012, before me, a Notary for the state of Idaho, personally appeared Wendell Olson known, or identified to me to be the President, of the corporation that executed this instrument or the person who executed the instrument on behalf of said corporation, and acknowledged to me that such corporation executed the same.

IN WITNESS WHEREOF, I have hereto set my hand and affixed my official seal the date and year in this certificate first above written.



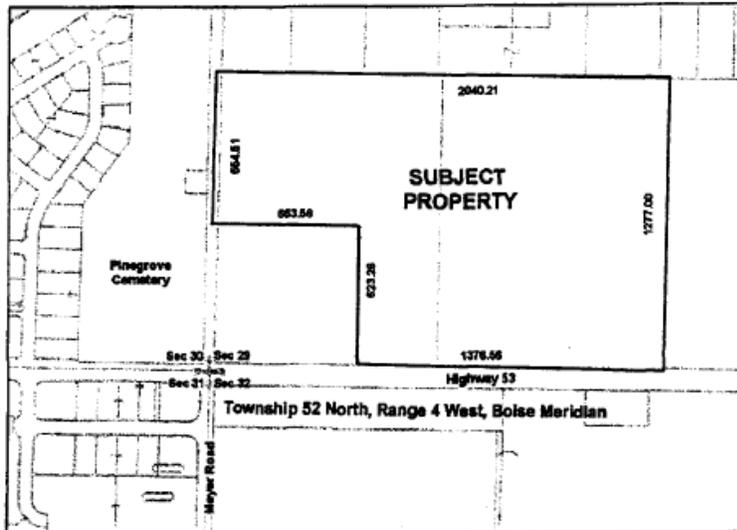
Kristen Olson  
Notary Public for the state of Idaho  
Residing at: Hayden, ID  
Commission Expires: 9/13/16

**EXHIBIT "A-1"**

**LEGAL DESCRIPTION**

A parcel of land being a portion of the South half of the Southwest Quarter of Section 29, Township 52 North, Range 4 West, Boise Meridian, Kootenai County, Idaho and being more particularly described as follows:  
**Commencing** at the Southwest corner of said Section 29 being a railroad spike per CP&F 1440304 from which the South Quarter corner bears S88°42'46"E a distance of 2654.35 feet; Thence, along the West line of said Quarter of the Southwest Quarter, N01°01'54"E a distance of 657.85 feet to the **True Point of Beginning**;  
Thence, continuing along said West line, N01°01'54"E a distance of 654.51 feet to a 3/4 inch iron pipe;  
Thence, leaving said West line, S88°50'32"E a distance of 2040.21 feet to a 5/8 inch rebar, 30 inches long with yellow plastic cap marked INC PLS 9367;  
Thence, S01°02'05"W a distance of 1277.00 feet to a 5/8 inch rebar, 30 inches long with yellow plastic cap marked INC PLS 9367 on the North right of way of State Highway 53;  
Thence, along said North right of way, N89°02'44"W a distance of 1376.56 feet to a 1/2 inch rebar with yellow plastic cap marked 772;  
Thence, leaving said North right of way, N01°01'54"E a distance of 623.28 feet to a point;  
Thence, N88°46'39"W a distance of 663.58 feet to the **True Point of Beginning**.  
Said parcel containing 50.460 acres of land, more or less.

**EXHIBIT "A-2"**





RATHDRUM  
PLANNING AND ZONING

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Exhibit G

**PLANNING AND ZONING COMMISSION MEETING**

**MEETING MINUTES**

Wednesday, January 15, 2020

6:00 PM

**PRESENT:** Commissioner Furey, Commissioner Carr, Commissioner Shuman

**ABSENT:** Commissioner Munyer and Commissioner Hatcher

**STAFF:** Planner Siess, Attorney Herrington and Assistant Planner Davey

**PLEDGE OF ALLEGIANCE**

**MINUTES:** Approval of the meeting minutes for December 18, 2019 as they are presented made in a motion by Commissioner Shuman and seconded by Commissioner Carr.

All in favor stating aye.

**VISITOR:** None

**PUBLIC HEARING:** Westwood Pines Subdivision

**Open the Public Hearing:**

Commissioner Furey stated the public hearing process.

Planner Siess gave an overview of the project with mentioning the location being north of Highway 53 and west of Meyer Road, a subdivision consisting of 113 lots on approximately 50 acres, the zoning being Residential R-2S for 110 lots with the average lot size being 8,995 square feet with two commercial lots. One lot located within the Residential R-3 and Residential R-2S zone district (split zoning) of approximately 8.8 acres will also be created. The plat will include dedication of right-of-way for extension of Meyer Road, and the other interior streets / sidewalks that will serve the development. Also, within the subdivision, one lot is dedicated to the City of Rathdrum for the purpose of a well to be constructed at a later date and with an easement to the east of the project along the R-3 zoning will be provided for a temporary sewer lift station.

Agency comments regarding the proposal were received from Northern Lakes Fire Department, Idaho Transportation Department and Department of Environmental Quality.

This parcel was annexed into the city limits in 2011 and subject to the Annexation Agreement recorded with the Kootenai County Recorder's office.

**Applicant:**

Ray Kimball of Whipple Consulting Engineers  
2104 Calmine CT  
Post Falls, ID 83854

Mr Kimball is representing Viking Construction and began by thanking the commissioners for their service on the board and acknowledged that their time is appreciated.

He stated the same information as the planner did above regarding location, number of lots, zoning, lot sized and the design of the streets within the subdivision. The traffic that will be generated by this subdivision will be accommodated by a traffic light that will be installed in the future at Meyer Road and Highway 53. They will be paving Meyer Road along their property with full utility infrastructure in all streets. It is currently planned to be developed in three phases. The sewer line that will supply the lift station will be ran along a street in the cemetery and will be paved.

Mr Kimball pointed out that the subdivision standards have been met with this submittal of Westwood Pines, being that it is in compliance with the city's comprehensive plan, street standards, drainage, city utilities and installing the temporary sewer lift station that is proposed in the Sewer Master Plan. Once the new sewer lift station is installed along Meyer Road south of this project, then this one will be taken offline.

Planner Siess gave an explanation of the two interior streets; Cloverdale Street and Meyer Road being that once there are more than 150 lots in a subdivision, there is a requirement per fire code to have two entrances to the subdivision.

Commissioner Furey asked if the developer was required to dedicate any 'green space' with this subdivision submittal? Planner Siess explained that in leu of donating property, that the city will collect a park impact fee when building permits are issued.

**Public testimony:**

Jim Dugan  
6302 W Mallory Road  
Rathdrum, ID 83858

Mr Dugan lives just north of the project and is concerned about his livestock and questioned whether or not the developer will install a perimeter fence around this project. He also commented that he uses Meyer Road for his main access and is concerned about the extra traffic this subdivision will generate.

John Leitzke  
5943 W Highway 53  
Rathdrum, ID 83858

Mr Leitzke would like to see fencing install around the property of the project and questioned just where the sewer lift station was going to be installed since he lives directly east of this subdivision.

Sally Williams  
5922 W Mallory Road  
Rathdrum, ID 83858

Ms Williams stated that she also lives just north of this project and would like to see a fence installed along the perimeter of the subdivision.

**Rebuttal:**

Mr Kimball stated that fencing has not been discussed by the developer, but they feel that most homeowners would put up their own fencing. Also, the developer has an agreement with the Lion's Club to tap their small RV park along Meyer Road with sewer stubs for each RV.

Richard Williams  
5922 W Mallory Road  
Rathdrum, ID 83858

Mr Williams lives just north of this proposed subdivision and is concerned about the survey being done for this project and how will affect his property and is worried about the safety of his horses.

**Close the Public Hearing**

Commissioners open discussion: they understand the concern of having a perimeter fence for safety and some sort of a buffer for neighboring properties. The traffic in this area of Highway 53 and Meyer Road is very congested already and with two schools (High School and Stem Academy) generating a lot of traffic. The need for a complete traffic study is required for such a congested area and the subdivision will be generating even more traffic. The application itself is in compliance with Rathdrum city code and everything seems to be in order.

Motion made by Commissioner Furey, to table this proposal, giving the applicant more time to do a traffic study and confer with the city engineer. Also, to address fencing around the property to make it more secure for the residence. Seconded by Commissioner Carr

Roll Call Vote:

Furey – aye

Shuman – aye

Carr – aye

All in favor stating aye

It is confirmed that this motion is tabled until the next regularly scheduled meeting, unless the applicant has the information requested earlier, then the city would schedule a special meeting with sufficient posting.

**NEW BUSINESS** – Food Trucks and Food Truck Courts

Planner Siess stated that the City of Rathdrum currently does not have a code to accommodate food trucks. Open discussion was done regarding food courts versus food trucks parking along side the road or parking lots. Permission from landowners to operate and the traffic flow that would be generated from both types. Allowing food trucks during special events without having to get a permit from the City. Requiring a Panhandle Health inspection along with a city permit. What to charge for a permit and how often to have them renewed. Commissioners asked what other jurisdiction did with this process and suggested that perhaps the City of Coeur d'Alene's planner could come to one of our meetings and give us some ideas on how to get started.

## **ADJOURNMENT**